

PROGRAMMATIC SECTION 4(f) EVALUATION

IDOT Region 2 / District 3

U.S. Route 52 (FAP 607) Fox River Crossing Project
Contract 66F85
LaSalle County, Illinois
Existing Structure No.: 050-0058



May 2025



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U.S. Department of Transportation
Federal Highway Administration

The Federal Highway Administration (FHWA) has determined that this project meets all requirements for processing under the Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges, approved on July 5, 1983. This determination is based on the attached documentation which has been independently evaluated by FHWA and determined to adequately and accurately discuss the Section 4(f) considerations of this project. Accordingly, FHWA gives Section 4(f) approval under the Programmatic Section 4(f) Evaluation for FHWA Projects that Necessitate the Use of Historic Bridges for the proposed replacement of the U.S. Route 52 Bridge over the Fox River (Structure No. 050-0058), which is eligible for listing on the National Register of Historic Places (NRHP).

6/2/2025

Date

Darien J. Siddall

For Federal Highway Administration

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1.0 INTRODUCTION AND APPLICABILITY

The Illinois Department of Transportation (IDOT) and the Federal Highway Administration (FHWA) propose to remove and replace the bridge (SN 050-0058) carrying U.S. Route 52 (US 52) over the Fox River located about 2.1 miles west of IL 71 in LaSalle County (see Exhibit 1). The project will consist of the removal and replacement of the existing bridge, as well as improvements to the adjoining roadway approaches.

SN 050-0058 is eligible for listing in the National Register of Historic Places (NRHP) and is therefore accorded protection under Section 4(f). The bridge was determined NRHP-eligible by IDOT Bureau of Design and Environment's Cultural Resource Unit on September 27, 2016, in their Historic Bridge Coordination memo (see Appendix C). This bridge replacement project is following the Section 4(f) process due to the structure being eligible for listing on the NRHP.

This report contains information describing the existing bridge features, its current condition, the project's purpose and need, alternatives considered to avoid adverse effects on the existing bridge, and measures to minimize harm proposed for the use.

The "Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges" is applicable to this project based on the following criteria:

1. The bridge is to be replaced or rehabilitated with Federal funds.
2. The project will require the use of a historic bridge which is on or is eligible for listing on the NRHP.
3. The bridge is not a National Historic Landmark (NHL).
4. The FHWA Division Administrator determines that the facts of the project match those set forth in the sections of this document labeled Alternatives, Findings, and Measures to Minimize Harm.
5. Agreement among the FHWA, the State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation (ACHP) has been reached through procedures pursuant to Section 106 of the National Historic Preservation Act (NHPA).

2.0 PROJECT BACKGROUND AND DESCRIPTION

2.1 History of the Bridge

The structure was originally built in 1931 as a five span, reinforced concrete arch bridge. In 1980, the original structure underwent a major rehabilitation. The deck, floor beams and spandrel columns above the arches and pier caps were completely removed and reconstructed. The existing stub abutments and wingwalls were removed and replaced with vaulted abutments. The profile was raised at the west end of the bridge which increased the elevation of the profile grade line at the front face at the west and east abutment by 1.61' and 0.47', respectively. The deck width increased to 35'-2" out-to-out. The decorative concrete parapets were replaced with F-shaped parapets. Concrete patching and crack injection repairs were also made to the arches and noses of the piers. Existing bridge plans are provided in Exhibit 2.

2.2 Description of the Bridge

The bridge structure (SN 050-0058) is a reinforced concrete open spandrel deck arch supported on reinforced concrete vaulted abutments and four intermediate reinforced concrete piers. The structure has no skew. The bridge length measures 459'-0" back-to-back of abutments consisting of five (5) main spans over the Fox River with two (2) approach spans above the vaulted abutments. The main spans are spandrel arch measuring 77'-9" long for the two (2) outside spans and 80'-0" long for the three (3) inside spans. The approach spans are 31'-9" long, measured from back to front of the vaulted abutments.

The width of the bridge measures 35'-2" out-to-out of deck and 33'-2" face-to-face of parapets. The existing bridge deck is 7 1/2" thick of reinforced concrete and provides a clear bridge width of 32' between the parapet curbs consisting of two 12' travel lanes with a 4' shoulder in each direction.

The Fox River is not considered a navigable waterway. The project limits are approximately 400 feet to the west and 600 feet to the east from the bridge. This section of US 52 is an undivided rural two-way, two-lane state-maintained route which is designated as a Class II truck route.

2.3 Purpose and Need of the Proposed Project

US 52 is a 180-mile arterial route in the north central and eastern portions of the State of Illinois that extends from the Savanna-Sabula Bridge over the Mississippi River on the Iowa State line in Carroll County to the Indiana State line near Sheldon in Iroquois County. Since being built, the bridge which carries US 52 over the Fox River has provided a reliable and safe crossing for more than 90+ years. It is an essential transportation link connecting and providing access to several small rural communities in the area. Both US 52 and the bridge are under the jurisdiction of IDOT.

The bridge carries approximately 2,300 vehicles per day (2021). The nearest river crossings are located approximately 3.1 miles north in Sheridan (FAS 1271 – Bowen Rd) and approximately 5.7 miles southwest in Wedron (FAS 2258 – E 2153 Rd). The shortest alternate state detour route is on Interstate 80 and involves approximately 13.09 miles of adverse travel (see Exhibit 3).

Crash data was evaluated between 2017 and 2021 and showed a total of seven (7) crashes near the bridge (see Exhibit 4). Of these reported crashes, one (1) involved striking a fixed object during winter weather conditions resulting in a property damage only (PDO). The remaining crashes did not occur on the bridge.

The purpose of this project is to improve the existing US 52 bridge crossing over the Fox River while minimizing impacts to the traveling public and its surrounding environment. The need for this project is because 1) the bridge is nearing the end of its service life, 2) the bridge is load posted, and 3) the bridge profile does not meet current IDOT geometric standards.

Bridge Condition: Based on the inspection (dated 08/26/2022), all the bridge components (deck, superstructure, and substructure) are rated as a 4 (poor condition with advance deterioration) categorizing the bridge as structurally deficient. The deck surface and soffit show areas of cracking, spalling, and delamination. Chloride concentration levels found on the deck has exceeded the corrosion initiation level at least 5" below the surface deck. The arches show widespread deterioration including large areas of spalls and cracks with exposed reinforcements, and delaminated areas on top and bottom of the arches. The arches are also lower than the existing deck drain extensions which allows for frequent exposure to salt laden water. The substructure elements are over 90 years old, and deterioration at the abutments and piers are becoming more prevalent.

Exposure to traffic loads, weather, and salt applications have (over time) caused continued deterioration to the structural elements of the bridge, especially to the deck and substructure. This rate of deterioration is evident when comparing past inspections from November 2016 to June 2022. In seven years, the condition rating of the deck and substructure has exacerbated from satisfactory with minor deterioration to poor with advance deterioration. This significant change in rating is an indication that the bridge is quickly approaching the end of its repairable life and nearing a point where it cannot reasonably be maintained for an extended period.

Since then, the overall sufficiency rating of the bridge has dropped from 64.3 (as of 03/30/2021) to 33.6 (as of 09/10/2024) based on the latest structure report. The load rating was updated on 01/27/2023 which shows an inventory and operating rating of 0.330 (11 tons) and 1.030 (37 tons), respectively. See Appendix B for the structure report and bridge photos.

Structural Adequacy: When the original structure was built in 1931, the bridge width was only 24 feet wide. The original structure was most likely designed to carry 40,000 pounds of loading, which is far less than the current design loadings (between 72,000 and 80,000 pounds). As mentioned before, the original structure underwent a major rehabilitation replacing the superstructure with a much wider deck in 1980. A load rating inspection was recently completed in May 2022 which concluded that the original design of the bridge did not account for certain limit state checks that are required by today's standards. A load posted sign was installed on 02/02/2023 showing an allowable weight limit restriction of 29 tons for single vehicle and 39 tons for combination vehicle. The required load posting on the bridge will limit truck traffic (11.3%) as well as large farming equipment.

Geometric Deficiency: The existing vertical profile near the west end of the bridge does not meet current department policy per the 1978 as-built plans. Based on a design speed of 55 mph, a minimum stopping sight distance of at least 495 feet is required according to IDOT Bureau of Design and Environment (BDE) manual. However, the existing vertical profile provides approximately 459 feet of stopping sight distance. In other words, a vehicle would not have enough distance necessary to stop before avoiding a hazard when it is detected. This is a safety concern for normal traffic operations as well as during future rehabilitations or repairs.

3.0 ALTERNATIVE AND FINDINGS

Four alternatives were evaluated to fulfill the purpose and need of the project and to determine whether each alternative is feasible and prudent. Although the structure is designated as historic, it still must meet current regulatory standards to remain a functional part of the transportation system and ensure public safety. The alternatives are listed below and discussed in detail.

3.1 Alternative 1A: Do Nothing

Under the “Do Nothing” alternative, the existing US 52 bridge would remain in place with no improvements made and its historic features would be retained. Only routine repairs and maintenance would be provided as needed.

Finding(s): This alternative does not satisfy the purpose and need of the project, and it is not feasible and prudent for the following reason(s):

- ❖ Maintenance – Normal or routine maintenance will not address the structure’s ongoing deteriorated and exacerbated condition. Repairs alone will not address the deficiencies that can lead to major structural problems for the remaining life of the bridge. As it continues to age, more frequent repairs will be needed to keep the bridge functional and safe which poses a serious disruption on established travel patterns.
- ❖ Safety – The “Do Nothing” alternative ignores the substandard geometric profile of the bridge. US 52 traffic is anticipated to increase over time. If the profile is not corrected or remains in place without providing the required stopping sight distance, the bridge could potentially pose serious and unacceptable safety hazards to the traveling public or places intolerable restriction on transport and travel.

3.2 Alternative 1B: Rehabilitation Without Affecting the Historic Integrity of the Bridge

Under Alternative 1B, the existing US 52 bridge would undergo a major rehabilitation to address its structural deficiencies and would allow the bridge to retain its historic integrity.

Finding(s): A Bridge Condition Report (BCR) was approved on 10/11/2017 which showed two options to rehabilitate the bridge while preserving its historic features. Option one (1) would repair the arches and apply a new deck overlay, and option two (2) would completely remove and replace the superstructure component with repairs made to the arches. These rehabilitation measures can preserve and extend the useful life of the bridge, but, for one or more of the following reasons, this alternative is not feasible and prudent:

- ❖ The bridge is so structurally deficient that rehabilitation or repairs made to the deck, superstructure, and substructure will not effectively meet the current minimum acceptable load requirements without affecting the historic integrity of the bridge. Rehabilitating the deck or replacing the superstructure is cost prohibitive since a significant portion of the anticipated work would come from repairing the arches and substructure. These repairs are likely to have limited lifespan and its effectiveness is uncertain if it can restore the structural capacity of the bridge.
- ❖ The bridge cannot be widened to meet future minimum required capacity on US 52 without affecting the historic integrity of the bridge. Because the condition of bridge is seriously deficient, a robust structural analysis will be required for the entire bridge to ensure its capacity can safely handle the additional loading. Widening the bridge would cost substantially more and afford a shorter life span as compared to replacement with a new bridge.

3.3 Alternative 1C: Build on New Location Without Using the Old Bridge

Under Alternative 1C, a new bridge would be constructed on a new alignment at a different location parallel to the existing bridge. The existing bridge would be closed as a transportation facility with service of all roadway transportation modes diverted to the new bridge location. This alternative would retain the historic integrity of the existing bridge. Exhibit 5 in Appendix A depicts the alignment for Alternative 1C.

Finding(s): This alternative does not satisfy the project's purpose and need and is not feasible and prudent for the following reason(s):

- ❖ *Terrain* – The existing bridge is already located at the most logical site across the Fox River. To build a new bridge at a different location will result in an immense bridge and roadway engineering effort and construction difficulty or costs, or significant disruptions to the existing travel patterns.
- ❖ *Adverse Social, Economic, or Environmental Effects* – Re-aligning US 52 to accommodate the new bridge across the Fox River would result in social, economic, or environmental impact. These negative impacts include, but are not limited to, acquiring significant amount of right-of-way, displacing residents, damaging wetlands and archeological areas, and disrupting the natural habitat of threatened and endangered (T&E) species for fish and mussel.
- ❖ *Engineering and Economy* – The immense engineering effort to construct the new bridge at a different location will be very difficult. It is expected that the roadway and structure cost in all phases of the project will reach extraordinary magnitude making this option cost prohibitive.
- ❖ *Preservation of Old Bridge* – It is not practical to maintain the existing bridge in a state of good repair, even if a new bridge were to be built at a new location. The longevity and effectiveness of the repairs that retains the structure's historic features or characteristics will be limited. The existing bridge will continue to exponentially deteriorate at a point when it is beyond rehabilitation for transportation use.

3.4 Alternative 1D: Remove and Replace the Existing Bridge

Under Alternative 1D, the existing bridge would be removed and replaced with a new bridge using the same alignment. It is anticipated that US 52 would be closed during construction and traffic will need to be maintained on a marked detour route.

This alternative is the most feasible and prudent alternative that fulfills the purpose and need of the project by improving the US 52 bridge crossing while minimizing impacts to the surrounding environment. The new bridge will have a much longer design life, no weight restrictions, and allow to bring the vertical profile up to current department policy. In the best interest of the public, Alternative 1D is the preferred option and will require the demolition and removal of the existing concrete arch deck, open-spandrel bridge which constitutes a Section 4(f) use.

4.0 Coordination and Public Involvement

In May 2023, letters were sent to seven parties to determine their interest in the project's adverse effect to historic properties and to invite them to be consulting parties. These parties are Historic Bridge Foundation, HistoricBridges.org, Landmarks Illinois, LaSalle County Historical Society, Sheridan Historical Society, Central Illinois Landmarks Foundation, and Northern Illinois Historic League. Copies of the letters and responses are included in Appendix D. Some parties have responded to date requesting consulting party status.

The project website is available at <https://idot.illinois.gov/transportation-system/transportation-management/featured-projects/u-s--52-in-lasalle-county---fox-river-bridge.html> which provides additional information about the project. A public notice was posted in News Tribune newspaper on August 28, 2023, and in The Times newspaper on September 7, 2023, for inclusion in the environmental documentation process. One public comment was received and is included in Appendix D.

Follow-up letters were sent to Landmarks Illinois and Northern Illinois Historic League to update them on the project status, and a letter was also sent to the private citizen who responded to the public notices.

See the historic group letters, public notices, summary of responses, and any public comment received in Appendix D.

5.0 Measures to Minimize Harm and Mitigation of Adverse Effect

5.1 Section 106 Commitments

On May 16, 2024, the SHPO concur with the determination that the proposed project will adversely affect the historic bridge (SN 050-0058). A Memorandum of Agreement (MOA) is being developed with SHPO for the removal and replacement of SN 050-0058 to resolve and mitigate the adverse effects. The MOA was executed by SHPO, IDOT, and FHWA and was finalized on [date]. A copy of the executed MOA is in Appendix C.

APPENDIX A

EXHIBITS

Project Location Map
As-Built Plans
Alternate Route / Detour Map
Crash Exhibits
Alternatives Considered

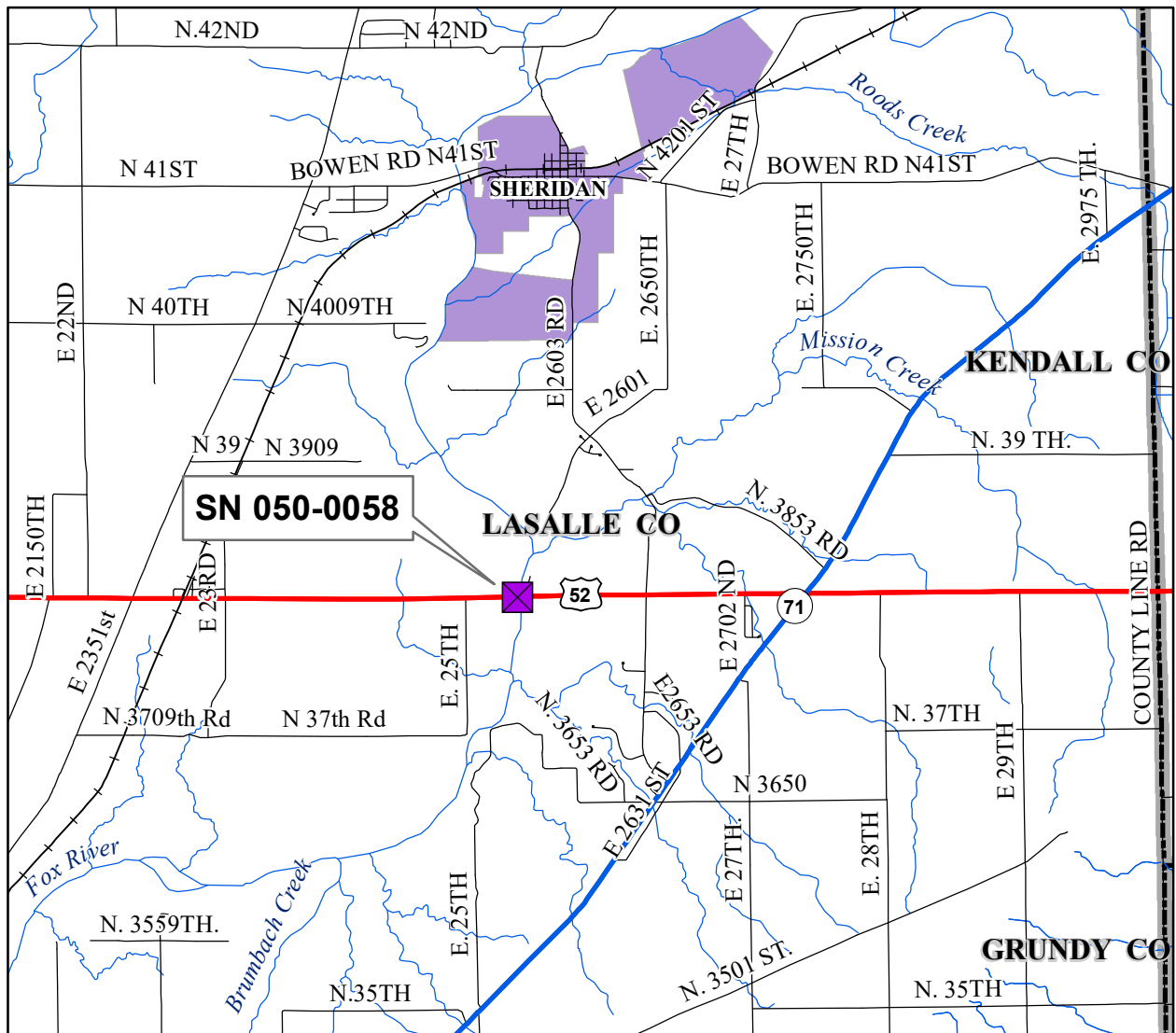
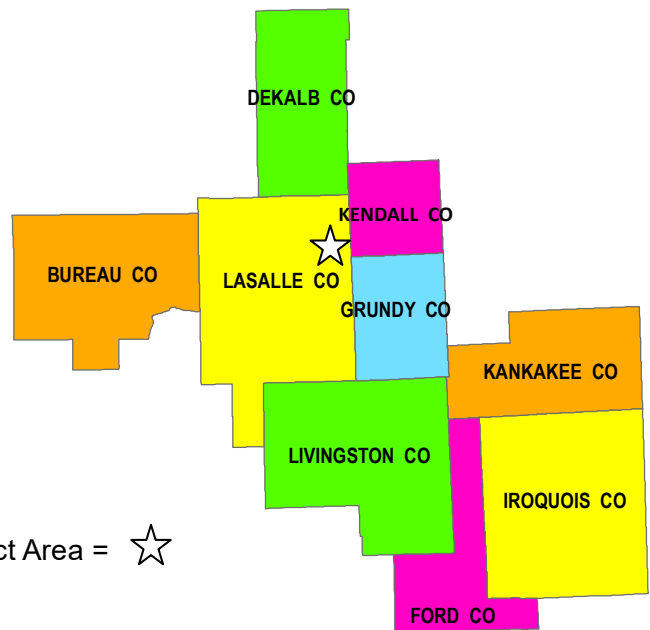
EXHIBIT 1 – Project Location Map

Project Location Map

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
SN 050-0058 over Fox River
2.1 miles west of IL 71
Phase I Job No: P-93-015-17
Contract No.66F85



Project Area = ☆



D3# 3115

EXHIBIT 2 – As-Built Plans

INDEX TO SHEETS

Sheet No	Title Page
1	Standard Cross Section 1367 & 1355
2	Standard Gutter Section 1309
3	Plan Showing Gravel Road Approach L. Sta 357180
4	Plan and Profile Sta. 226100 to Sta 280100
5	" " " " 280100 " " 340100
6	" " " " 340100 " " 370100
7	" " " " 370100 " " 400100
8	" " " " 400100 " " 460100
9	" " " " 460100 " " 520100
10	" " " " 520100 " " 580100
11	" " " " 580100 " " 598138
12-36	Inclusive Cross Sections.
37	Standard Culvert Design Nos. 828-1, 828-2, 1204-2.
38	Special Culvert Design Stations; 230100; 250100; 297175
39	" " " " 380100; 424100; 471128, 531135
40	" " " " 307152; 331147; 383138-4;
41	" " " " 401102; 411150; 491150; 578138-1 & 2
42	" " " " 315157; 461170; 471150; 510199, 561104
43	" " " " 584140.
44	Plan Showing Pnt. Const. adjacent Bridges
45	Standard 1283.
46	Standard 1162.

SECTION 125B

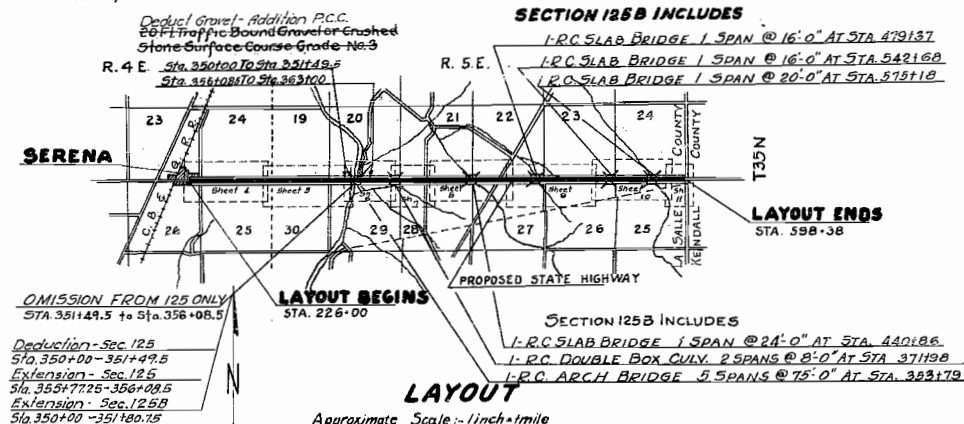
Sheet No	Title Page
1	Plan & Profile Sta 340100 to Sta 370100
2	" " " " 370100 " " 400100
3	" " " " 400100 " " 460100
4	" " " " 460100 " " 520100
5	" " " " 520100 " " 580100
6	Special Box Culvert Design, Sta. 371179 Sht 1 of 1 Sht.
7	Special Bridge Design, Sta. 393179 Sht 1 of 4 Shts.
8	" " " " " " 2 " " "
9	" " " " " " 3 " " "
10	" " " " " " 4 " " "
11	" " " " " " 440186 " " 1 " 2 "
12	" " " " " " 479137 " " 1 " 2 "
13	" " " " " " 542168 " " 1 " 2 "
14	" " " " " " 575118 " " 1 " 2 "
15	" " " " " " 2 " " "
16	Standard 1177

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS PLANS FOR PROPOSED STATE HIGHWAY

SCALES PLAN 1 INCH = 100 FT.
PROFILE - HOR. 1 INCH = 100 FT.
PROFILE - VERT. 1 INCH = 10 FT.
CROSS SECTIONS 1 INCH = 5 FT.

PROJ. 190 ROUTE 69, SECTION 125 & 125B, LASALLE CO.

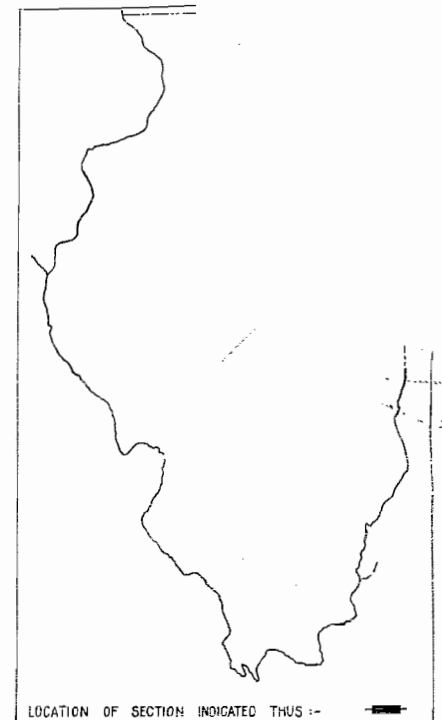
From a point near the SW Corner of the SE 1/4 of the SE 1/4 of Sec. 23, T. 33 N., R. 4 E. of the 3rd P.M.
To a point near the SE Corner of Section 24 T. 33 N. R. 5 E. at the 3rd P.M.



STATE AND NATIONAL LINE	---	LEVEE	---
COUNTY LINE	---	CULVERTS	---
CITY, VILLAGE OR BOROUGH	---	DROP INLET	---
TOWNSHIP LINE	---	TROLLEY POLE	---
SECTION LINE	---	POWER POLE	---
GRANT LINE	---	TELEPHONE OR TELEGRAPH POLE	---
FENCE LINE	---	MARSH	---
GUARD RAIL	---	HEDGE	---
UNFENCED PROPERTY	---	GROUND ELEVATION	---
RIGHT OF WAY LINE	---	GRADE ELEVATION	---
TRAVELED WAY	---	ELECTRIC RAILROADS	---
RAILROADS	---		
RETAINING WALL	---		
BASE OR SURVEY LINE	---		

LASALLE COUNTY SECTION 125 & 125B ROUTE 69 PROJ. 190

ROAD DIST.
ROUTE NO.
69
FED. ROAD



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
SUBMITTED: MARCH 5, 1931
DISTRICT ENGINEER
ENGINEER
PASSED: APRIL 20, 1931
ENGINEER OF DIST. EN.
APPROVED: APRIL 20, 1931
DISTRICT ENGINEER
CHIEF, Bureau of Public Roads

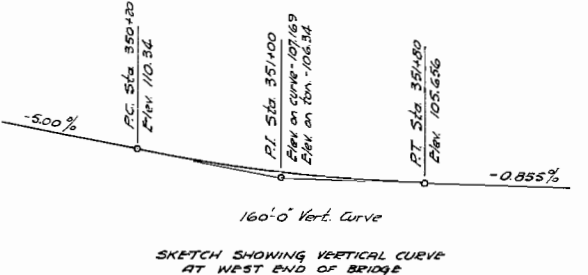
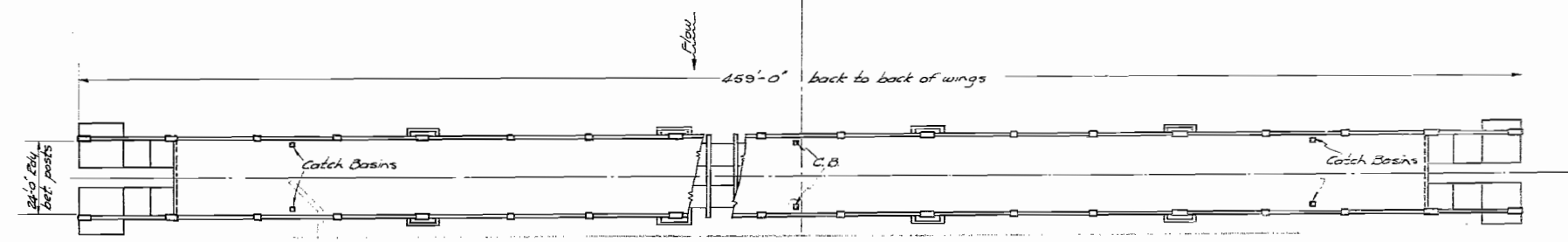
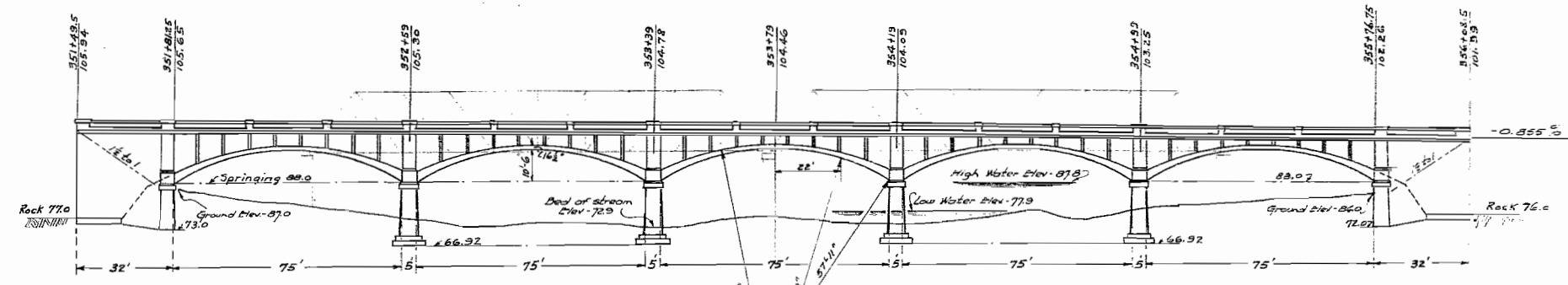
RECOMMENDED FOR APPROVAL
DISTRICT ENGINEER Bureau of Public Roads
RECOMMENDED FOR APPROVAL
CHIEF ENGINEER Bureau of Public Roads
APPROVED
CHIEF, Bureau of Public Roads

Made in Dist. Office 3-5-31
Rev. N.C.B. 4-10-31

Existing Structure -
Steel truss - Span - 2 @ 150'-0" - Roadway - 15'-0"
To be removed by bridge contractor.
Bench Mark -
#45 N. & W. Tree - Right Station 351+80
Elevation - 103.20

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SECTION	SEC	COUNTY	TOTAL	DATE	SHEET NO.
69	125B	LA SALLE	49	19	1
190					4 SHEETS



TOTAL BILL OF MATERIAL	
ITEM	TOTAL QUANTITY
Class A Concrete - Cu. Yds.	1040.9
Class X Concrete - Cu. Yds.	1403.7
Reinforcing Steel - Lbs.	155,570
4" P.C.C. Pavement - Sq. Yds.	1000.0
Catch Basins	6
Excavation (Rock) - Cu. Yds.	910
Name Plate	1

COMPUTED -	W. J. SUPERAK	EXAMINED -	5-16-31
CHECKED -	M. P. SUPERAK	DESIGNED -	W. J. SUPERAK
DRAWN -	W. J. SUPERAK	PASSED -	W. J. SUPERAK
CHECKED -	M. P. SUPERAK	APPROVED -	W. J. SUPERAK
SPECIAL ASSEMBLED -		CHIEF HIGHWAY ENGINEER	
CHECKED -			

FOX RIVER
S.D. 1. ROUTE 69 - SEC. 125-B
LA SALLE COUNTY
STATION 353+79

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

*FA RTE. 607 (U.S. RTE. 52) (S.B.I. RTE. 69)

* 125 BR. LASALLE 23 1
BR-F-607(27)

P-93-022-76

SHEET No.	INDEX OF SHEETS ITEM
1.	COVER SHEET
2.	TYPICAL SECTION & DETAILS
3.	SUMMARY OF QUANTITIES & SCHEDULES
4.	SCHEDULES
5.	PLAN & PROFILE
6-20.	BRIDGE PLANS
21-23.	X-SECTIONS

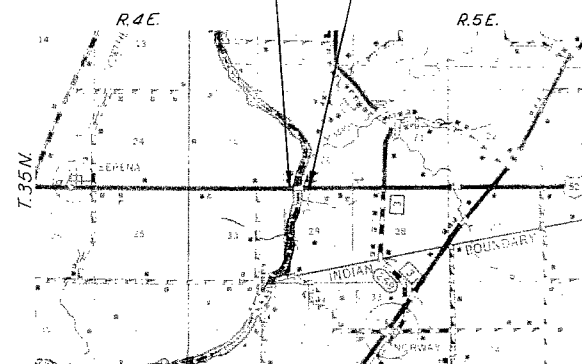
F.A. RTE. 607 (U.S. RTE. 52) (S.B.I. RTE. 69)
SECTION 125 BR.
PROJECT BR-F-607(27)
LASALLE COUNTY
C-93-076-78

STANDARDS

1686-4	Symbols & Abbreviations
1909-10	Bridge Approaches
1914-6	Type A Gutter
2113-1	Name Plate for Bridges
2228-4	Metal End Section for Pipe Culverts
2230-11	Steel Plate Beam Guardrail
2262-3	Precast Reinforced Concrete Flared End Section
2298-4	Typical Application of Traffic Control Devices
2299-7	Design of Traffic Control Devices
2300-1	Flagman Traffic Control Sign
2323-4	Pavement Joints
2324-3	Bridge Approach Shoulder Pavement
2336	Traffic Barrier Terminal Type I & IA
2341	Traffic Barrier Terminal Type 6
2362	Concrete Headwall for Pipe Drains

BEGIN PROJECT
STA. 348+70.00

END PROJECT
STA. 358+00.00



LOCATION MAP

SCALE IN MILES
0 mi. 1 mi. 2 mi.

DESIGN DESIGNATION:
825(93) AREA SERVICE 0.16(B-15)

12/5 78
R. L. Ba.
January 8 79
H. L. W. L.
January 8 79
Thomas L. Bright
January 8 79
J. W. Thompson

GROSS LENGTH=0.176 MILE=930.00 FEET
~ NET LENGTH=0.176 MILE=930.00 FEET

CONTRACT NO. 33605

LASALLE 125 BR. F.A. RTE. 607
(U.S. RTE. 52) (S.B.I. RTE. 69)

~SEC. 20, T. 35 N., R. 5 E., 3RD P.M.~

350

REMOVE EXISTING CABLE ROAD GUARD-90 LIN. FT. (LT. & RT.)
INSTALL TRAFFIC BARRIER TERMINAL TY. 6 (LT. & RT.)
INSTALL TRAFFIC BARRIER TERMINAL TY. 1 (LT. & RT.)
INSTALL 375 LIN. FT. SPBGR. TY. A (LT. & RT.)

355

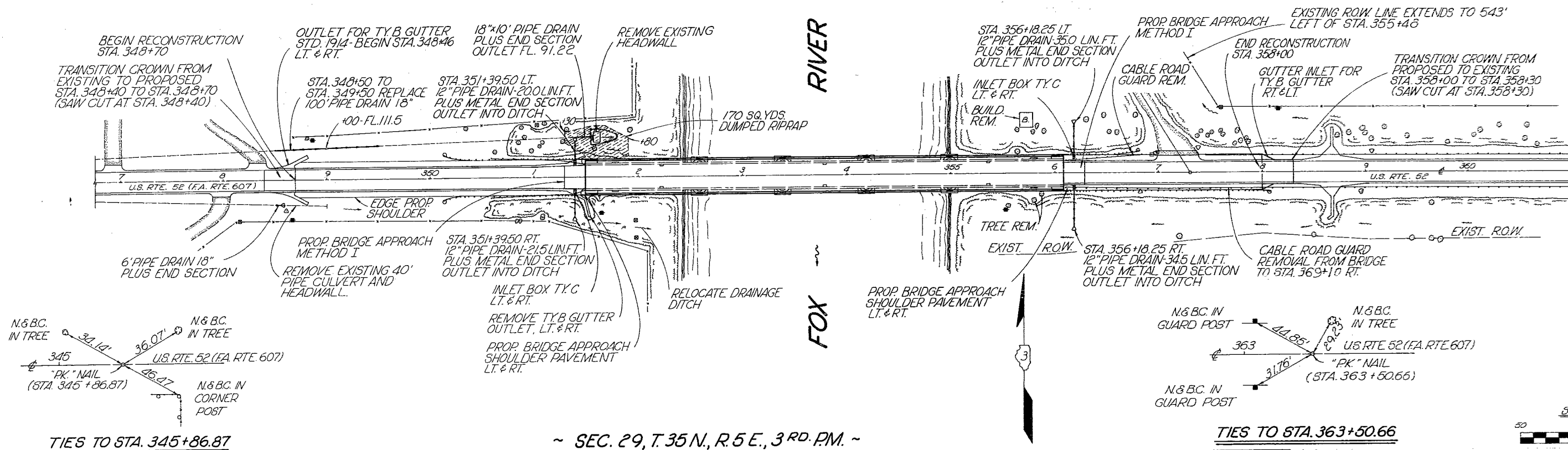
REMOVE EXISTING CABLE ROAD GUARD-74 LIN. FT. (LT.)
INSTALL TRAFFIC BARRIER TERMINAL TY. 6 (LT.)
INSTALL TRAFFIC BARRIER TERMINAL TY. 1 (LT.)
INSTALL 375 LIN. FT. SPBGR. TY. A (LT.)

360

FEDERAL AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
607	125BR	LASALLE	23	5
FEMA REGION 5 ILLINOIS PROJECT				

INSTALL TRAFFIC BARRIER TERMINAL TY. 6 (RT.)
INSTALL TRAFFIC BARRIER TERMINAL TY. 1A (RT.)
INSTALL 1300 LIN. FT. SPBGR. TY. A (RT.)

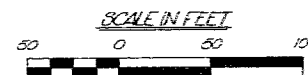
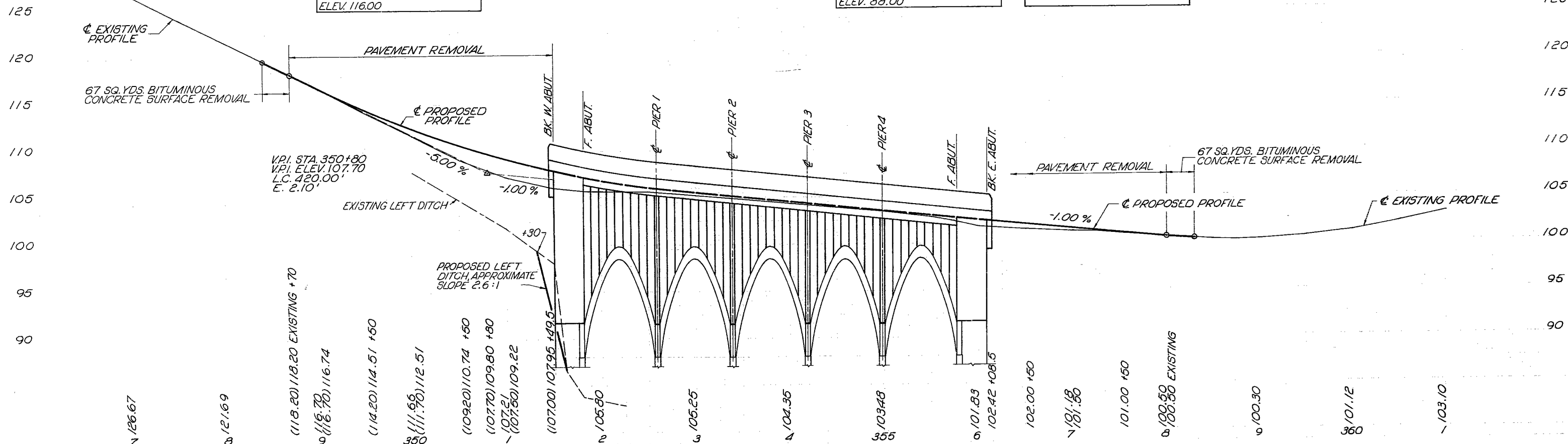
REMOVE CABLE ROAD GUARD STA. 357+45 TO STA. 364+95 LT. INSTALL 712.5 LIN. FT. SPBGR. TYPE A. INSTALL 2 TRAF. BARRIER TERMINAL TYPE 1A.



B.M. - CHISELED "D" ON NORTH END CONCRETE HEADWALL.
STA. 349+01.25' RT.
ELEV. 116.00

B.M. - SPRINGING LINE NORTH-WEST CORNER EAST ABUTMENT STA. 355+77, 13' LT.
ELEV. 88.00

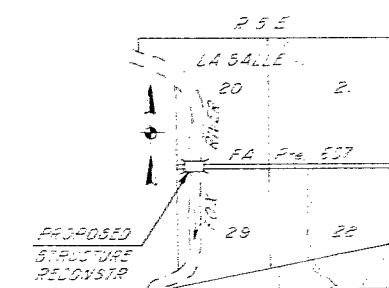
B.M. - RAILROAD SPIKE IN 27" TREE STA. 357+09, 75' LT.
ELEV. 98.02



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A. RT. 607	125 - BR	LA SALLE	23	6
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		

B.M. ELEV. 88.00: SPRINGING LINE
AT N.W. CORNER OF EAST ABUTMENT

EXISTING STRUCTURE WAS BUILT IN 1931 AS S.B.I. ROUTE 69
SECTION 125 B STA. 353+79. IT IS A 5 SPAN REINFORCED
CONCRETE OPEN SPANDREL ARCH ON SOLID PIERS AND CLOSED
ABUTMENTS.



LOCATION PLAN

STATION 353+79
BUILT 1931 BY
STATE OF ILLINOIS
F.A. RT. 607 SEC. 125 BR
F.A. PROJ. 125-125 BR
LOADING H20
STR. NO. 050-0050

NAME PLATE
See Standard 2113
(Locate Name Plate as directed
by the Engineer)

DESIGN SPECIFICATIONS

1971 A.A.S.H.T.O. AS APPLICABLE

DESIGN STRESSES

f'_c = 3,500 psi DECK SLAB, CURB, PARAPET,
COLUMNS, AND FLOOR BEAMS
 f_y = 60,000 psi REINFORCEMENT

LOADING

H5 20-44 (NEW CONSTRUCTION)
25 ft. FUTURE WEARING SURFACE

DESIGN METHOD

LOAD FACTOR DESIGN

APPROVAL

FOR STRUCTURAL ADEQUACY ONLY

ENGINEER'S SIGNATURE

DATE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A. RTE. 607 - (U.S. RTE. 52)
OVER

FOX RIVER

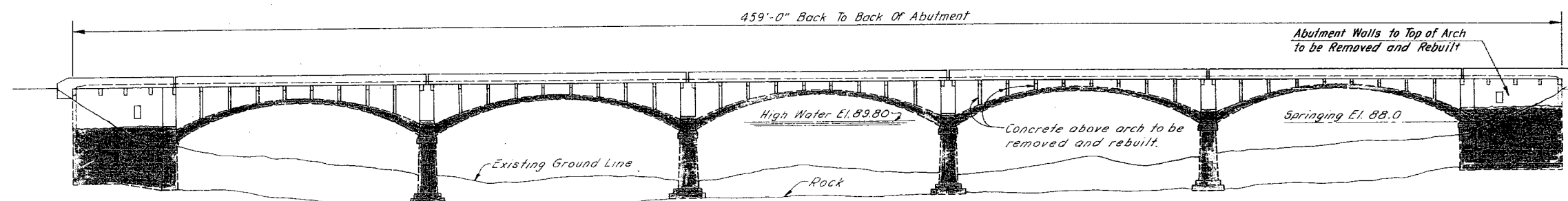
STA. 353+79

GENERAL PLAN

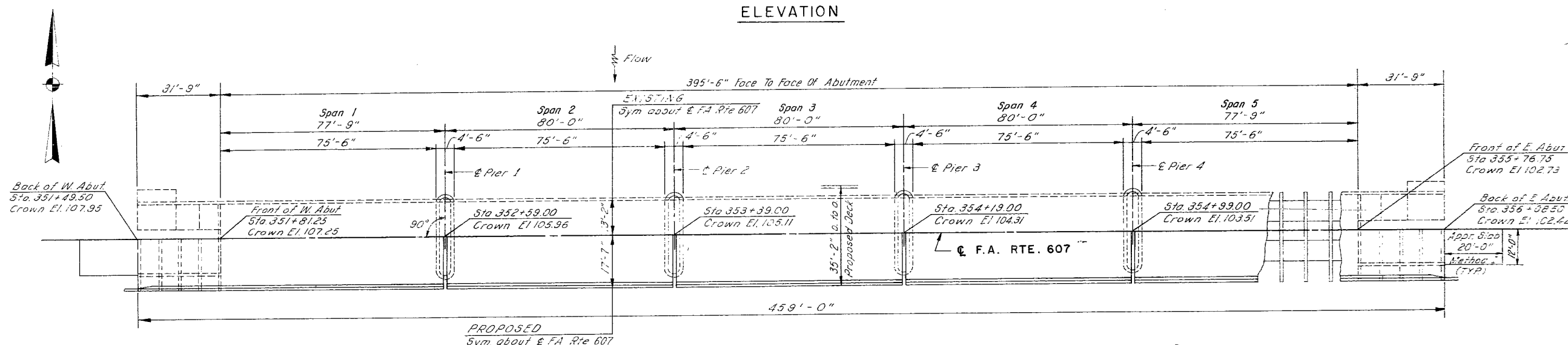
F.A. RT. 607 LA SALLE COUNTY SECTION 125 - BR

CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

SHEET
1 of 15



ELEVATION



PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPERST.	SUBSTR.	TOTAL
* Concrete Removal ①	Cu. Yds.	125.7	132.9	258.6
Expansion Bolts 3/4" x 12" Inch	Each	—	178	178
Protective Coat	Sq. Yds.	377	—	377
Class X Concrete	Cu. Yds.	791.3	141.9	933.2
Pressure Injecting Cracks	Lin. Ft.	—	142	142
Structural Steel	Lbs.	5458	—	5458
Reinforcement Bars	Lbs.	118,540	13,090	131,630
Reinforcement Bars Epoxy Coated	Lbs.	71,520	—	71,520
Name Plates	Each	1	—	1
Epoxy Grouting (1" x 3")	Sq. Ft.	—	336	336
Preformed Joint Sealer 2 1/2" Inch	Lin. Ft.	210	—	210
Expansion Bolts 1/2" x 6"	Each	—	444	444
Roadway Excavation	Cu. Yds.	—	—	—
Porous Granular Embankment	Cu. Yds.	—	271	271
Pipe Underdrain 6"	Lin. Ft.	—	135	135
** Removal of Concrete Deck ①	L.S.	1	—	1
Pipe Drain 6"	Lin. Ft.	—	24	24
Concrete Headwall for Pipe Drain	Each	4	—	4
Concrete Removal (Partial Depth)	Cu. Yds.	—	26.3	26.3
Concrete Patching (Partial Depth)	Cu. Yds.	—	26.3	26.3

*Includes Columns, Curtain Walls & Abutments.

**Includes Parapets, Slab & Floor Beams.

① Plans of the existing structure are available
at the office of the District Engineer.

DESIGNED BY: A.T.
DRAWN BY: V.P.
CHECKED BY: A.T.

GENERAL NOTES

CALCULATED WEIGHT OF STRUCTURAL STEEL = 5458 LBS.

ALL STRUCTURAL STEEL SHALL BE SHOP PAINTED WITH TWO COATS OF
BASIC LEAD SILICO CHROMATE PAINT.

THE CONTRACTOR SHALL MAKE ALLOWANCE FOR THE DEFECTION OF FORMS,
SHRINKAGE AND SETTLEMENT OF FALSEWORK, IN ADDITION TO ALLOWANCE
FOR DEAD LOAD DEFLECTION.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL
DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO
CONSTRUCTION AND ORDERING OF MATERIALS.

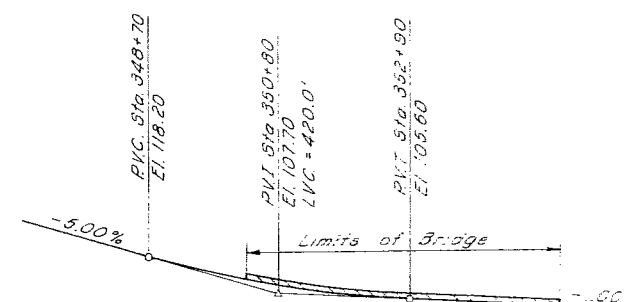
THE CONCRETE RAIL SECTION ABOVE THE BONDED CONSTRUCTION JOINT
AT THE TOP OF THE SLAB SHALL BE CONSTRUCTED OF CLASS X CONCRETE,
EXCEPT THE AGGREGATES SHALL CONFORM TO THE REQUIREMENTS OF
HANDRAIL CONCRETE.

EXPANSION BOLTS SHALL CONSIST OF SELF DRILLING EXPANSION ANCHORS
AND 3/4" x 12" HOOKED BOLTS.

ALL REINFORCEMENT BARS SHALL CONFORM TO A.A.S.H.T.O. M-31
GRADE 60 OR M-53 GRADE 60.

SEQUENCE OF REMOVAL AND CONSTRUCTION OF THE SUPERSTRUCTURE IN ARCH SPANS

- 1- THE REMOVAL AND RECONSTRUCTION OF THE SUPERSTRUCTURE SHALL
BE MADE IN SUCH A MANNER THAT SYMMETRY OF LOADING IS MAINTAINED.
- 2- THE CONTRACTOR SHALL SUBMIT REMOVAL AND CONSTRUCTION
PROCEDURES FOR APPROVAL BY THE ENGINEER PRIOR TO THE REMOVAL
AND CONSTRUCTION OF THE SUPERSTRUCTURE.



PROFILE GRADE ALONG
FA ROUTE 607

WATERWAY INFORMATION

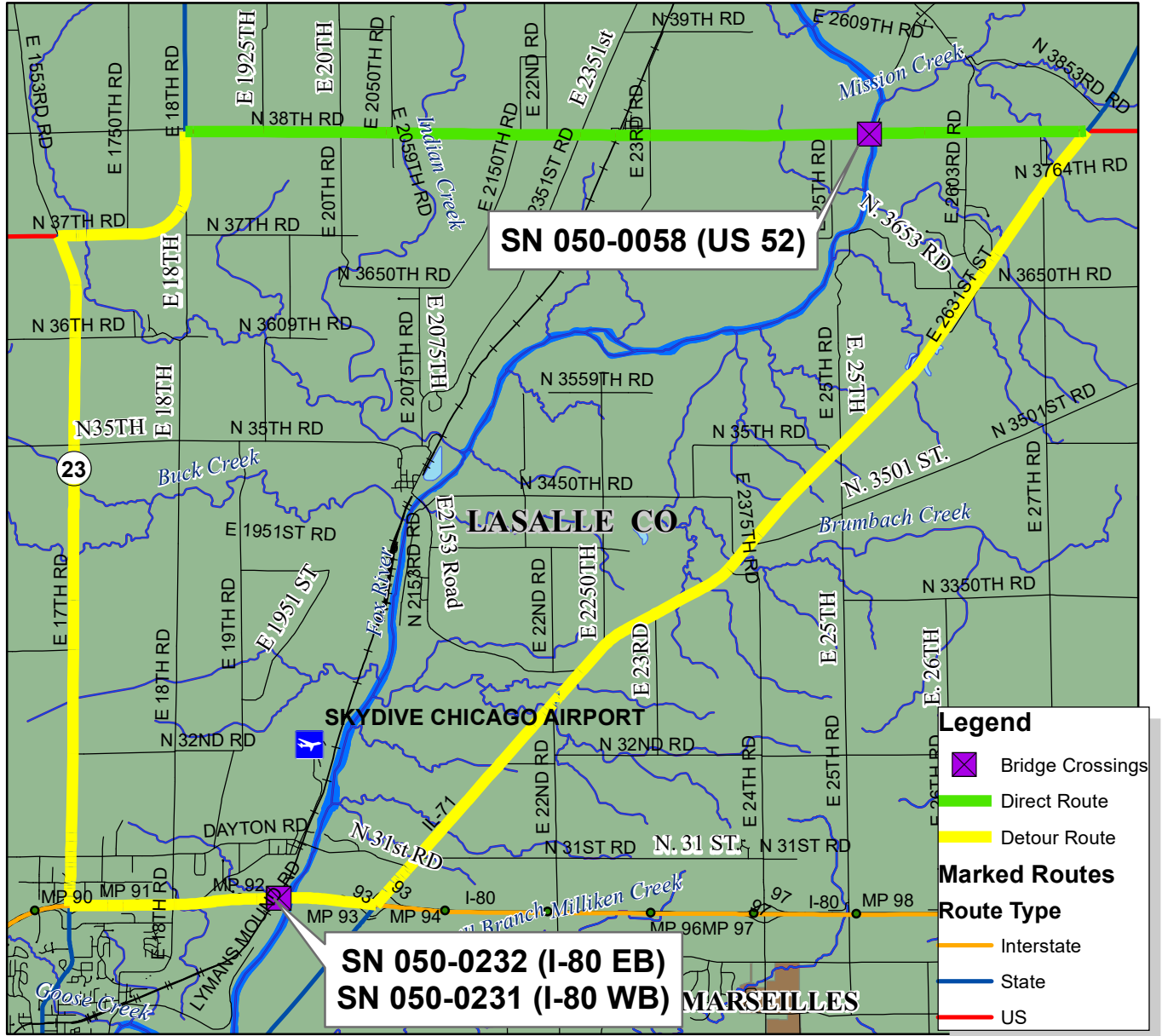
DRAINAGE AREA	2260 SQ. MI.
REQUIRED OPENING	3720 SQ. FT.
EXISTING OPENING	3720 SQ. FT.
PROPOSED OPENING	3720 SQ. FT.

	U.S.G.S.	LOCAL DATUM
Q50 =	21,425 c.f.s.	H.W.E. 531.65
Q100 =	24,560 c.f.s.	H.W.E. 532.30
DESIGN =		H.W.E. 531.65
ALL TIME		H.W.E. 535.60
CREATED HEAD FOR DESIGN FLOOD 0.38'		93.75 (1954)
CREATED HEAD FOR 100 YEAR FLOOD 0.60'		

EXHIBIT 3 – Alternate Route / Detour Map

State Detour Route

Marked Route - SN 050-0058



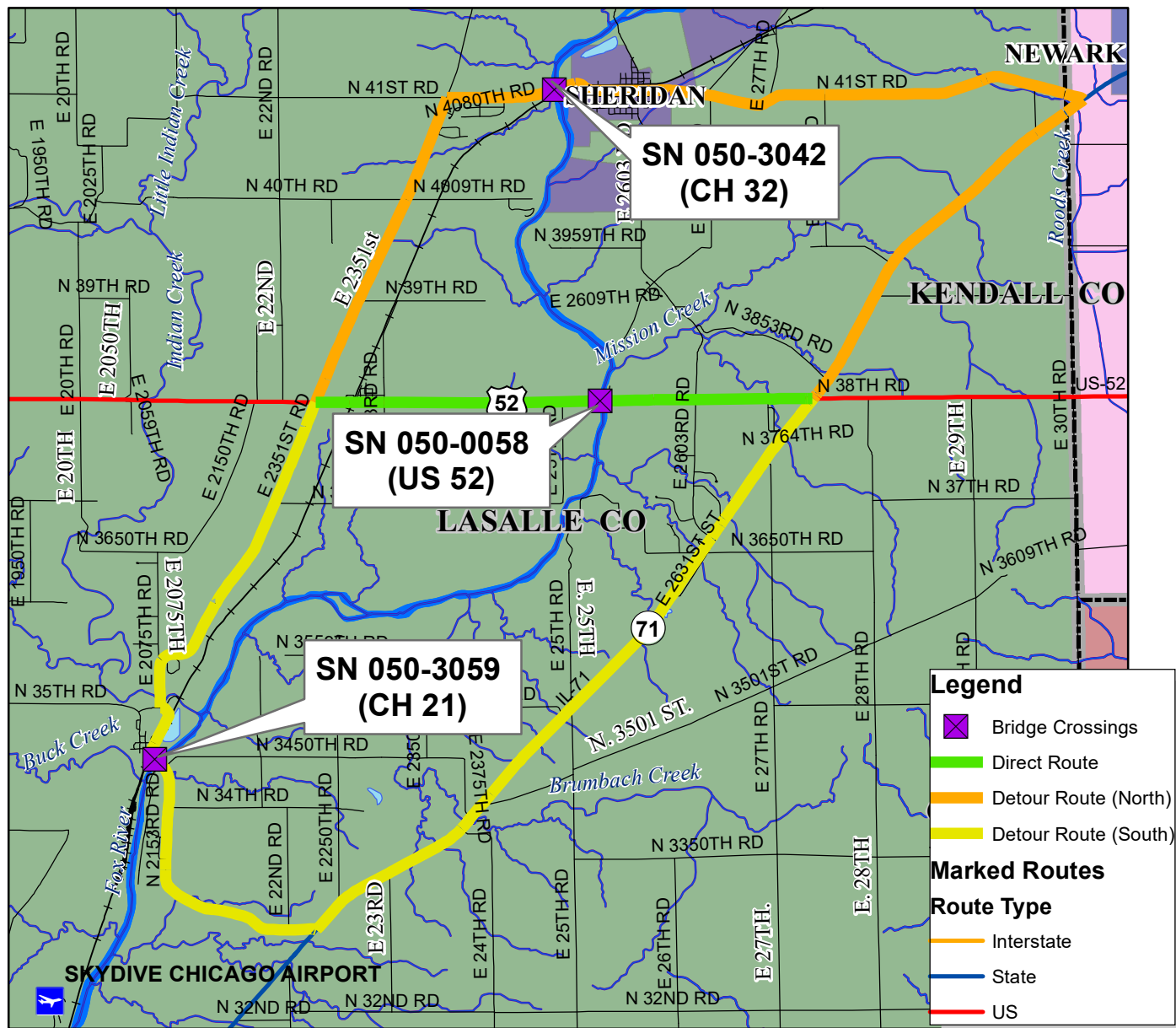
Direct Route Distance: 8.76 miles

Detour Route Distance: 21.85 miles

Adverse Travel: 13.09 miles

State and Local Detour Route

Marked Route - SN 050-0058



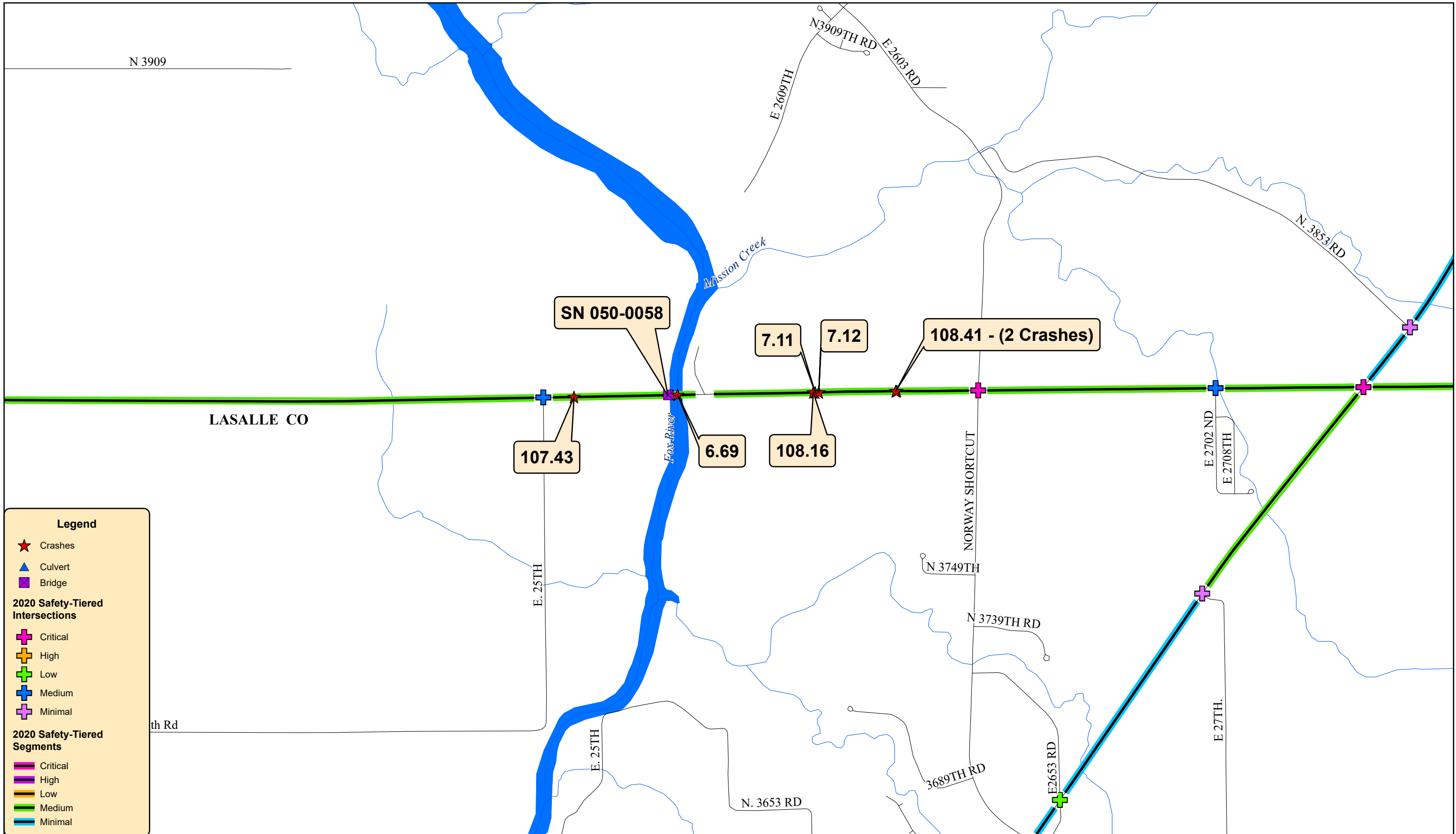
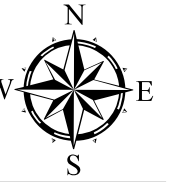
Direct Route Distance: 4.92 miles

Detour Route Distance: 13.71 miles (North) / 14.38 miles (South)

Adverse Travel: 8.79 miles (North) / 9.46 miles (South)

FOR INFORMATION ONLY

EXHIBIT 4 – Crash Exhibits

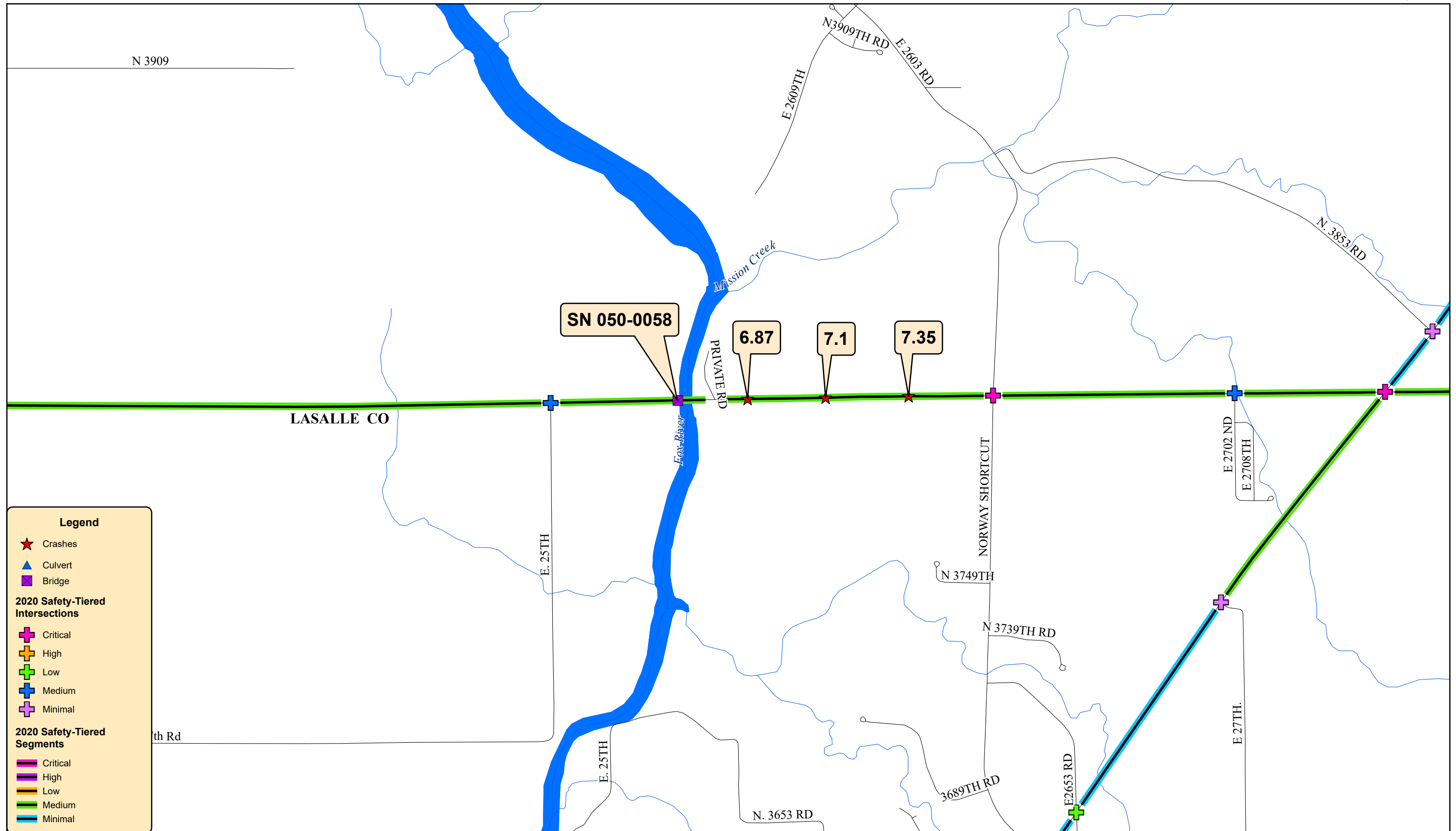
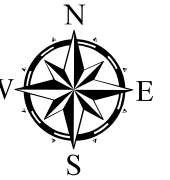


Route	Mile	Case ID#	Collision Type	Surface/ Weather	Crash Date Time yyyy-mm-dd hh:mm:ss	Direction	Driver Condition	Vehicle Type	Maneuver Code	Fatal Count	Injury Count	Injury Type	Lighting
US052	7.11	202001041825	Animal	Dry Clear	2020-01-26 18:30:00 V1 V2	West	Normal	SUV	Straight Ahead	0	0	PD	Darkness
US052	6.69	201901212666	Fixed Object	Snow or Slush Snow	2019-04-14 08:21:00 V1 V2	East	Normal	Passenger	Skidding/Control Loss	0	0	PD	Daylight
US052	7.12	201901446098	Animal	Wet Rain	2019-11-30 18:29:00 V1 V2	East	Normal	SUV	Straight Ahead	0	0	PD	Darkness
US052	107.43	201801423591	Other Object	Dry Clear	2018-02-26 08:48:00 V1 V2	East	Normal	Passenger	Straight Ahead	0	0	PD	Daylight
US052	108.41	201801452065	Fixed Object	Dry Clear	2018-07-03 18:16:00 V1 V2	West	Normal	Passenger	Skidding/Control Loss	0	1	B-Injury	Daylight
US052	108.16	201801491518	Animal	Dry Clear	2018-12-02 18:30:00 V1 V2	West	Normal	Passenger	Straight Ahead	0	0	PD	Darkness
US052	108.41	201701460536	Fixed Object	Dry Clear		V1 V2 West	Normal	Passenger	Skidding/Control Loss	0	1	B-Injury	Daylight

Total Injuries: 2

Total Fatalities: 0

Total Crashes: 7



Route	Mile	Case ID#	Collision Type	Surface/ Weather	Crash Date Time <small>yyyy-mm-dd hh:mm:ss</small>	Direction	Driver Condition	Vehicle Type	Maneuver Code	Fatal Count	Injury Count	Injury Type	Lighting
US052	6.87	202201053147	Animal	Dry Clear	2022-02-07 00:05:00 V1 V2	East	Normal	Pickup	Straight Ahead	0	0	PD	Darkness
US052	7.35	202201379542	Fixed Object	Dry	2022-10-23 03:56:00 V1 V2	East	Normal	Passenger	Avoiding Vehicle/Objects	0	1	A-Injury	Darkness
US052	7.1	202201435931	Fixed Object	Snow or Slush Snow	2022-12-22 21:30:00 V1 V2	West	Other/Unknown	Passenger	Unknown	0	0	PD	

Total Injuries: 1

Total Fatalities: 0

Total Crashes: 3

EXHIBIT 5 – Alternatives Considered

Alternatives Considered

SN 050-0058 Carrying US 52 Over Fox River

Contract #66F85 - LaSalle County

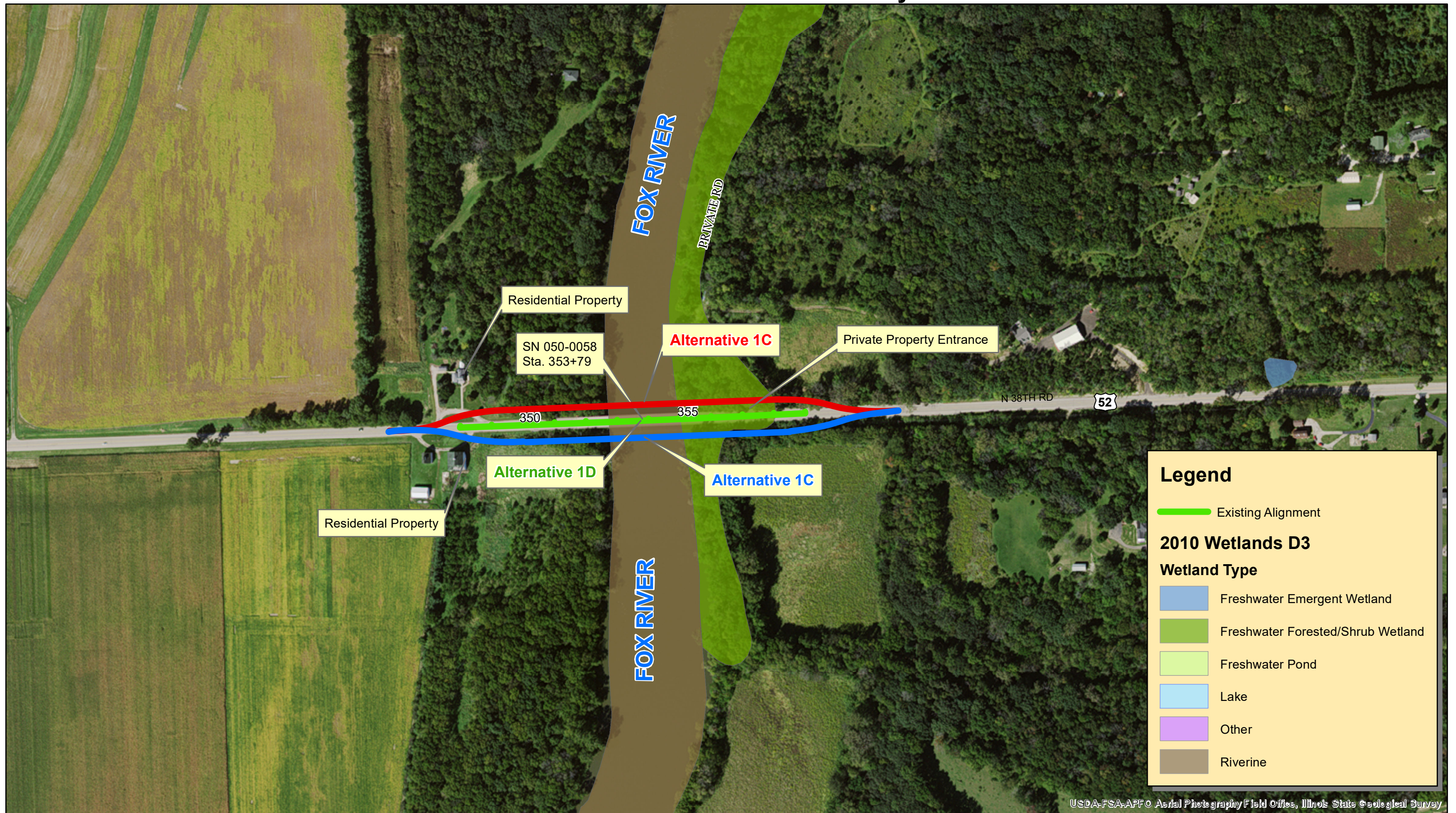
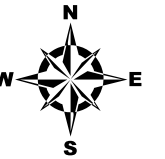


Image Source: NE-Illinois08_1ft.ecw. Flown in 2008

0 50 100 200 300 400
Feet
1 inch = 291 feet

APPENDIX B

STRUCTURE REPORTS & BRIDGE PHOTOS

**Illinois Department of Transportation
Structures Information Management System
Structure Summary Report**

Date: 01/04/2017

Page: 1

Structure Number: 050-0058

District: 3

Inventory Data

Facility Carried:	US ROUTE 52	Bridge Name:		Sufficiency Rating:	68.5	Structure Length:	459.0		
Feature Crossed:	FOX RIVER	Location:	2.10 MI W OF ILL 71	HBP Eligible:	Yes	AASHTO Bridge Length:	99.9		
Bridge Remarks:				Replaced By:	-	Length of Long Span:	80.0		
Bridge Status:	1 OPEN - NO RESTRICT	Status Date:	04/1988	Replaces:	-	Bridge Roadway Width:	32.0		
Status Remarks:				Last Update Date:	07/05/2012	Appr Roadway Width:	24.0		
Maint County:	050 LASALLE	Maint Township:	31 SERENA	Parallel Structure:	None	Deck Width:	35.0		
Maint Responsibility:	01 I.D.O.T.			Multi-Level Structure Nbr:		Sidewalk Width Right:	0.0		
Service On/Under:	1 HIGHWAY	5 / WATERWAY		Skew Direction:	N None	Sidewalk Width Left:	0.0		
Reporting Agency:	1 I.D.O.T. - BUREAU OF MAINTENANCE			Skew Angle:	0 D	Navigation Control:	0 No		
Main Span Matl/Type:	1 CONCRETE	/ 25 ARCH-DECK, OPEN SPANDREL		Structure Flared:	No	Navigation Horiz Clear:	0		
Nbr Of Main Spans:	5	Nbr Of Approach Spans:	2	Historical Significance:	Yes	Navigation Vert Clear:	0		
Approaches				Border Bridge State:		Culvert Fill Depth:	0.0		
Near #1 Matl/Type:	1 CONCRETE	/ 01 SLAB		Bdr State SN:		Number Culvert Celis:	0		
Near #2 Matl/Type:		/		Bdr State % Responsibility:	0	Culvert Opening Area:	0.0		
Far #1 Matl/Type:	1 CONCRETE	/ 01 SLAB		Structural Steel Wt	0	Culvert Cell Height:	0.00		
Far #2 Matl/Type:		/		Substructure Material:		Culvert Cell Width:	0.00		
Median Width/Type:	0 Ft. / 0	None		Rated By:	2 IDOT	Rate Method:	6 LOAD FACTOR (LF) REPORTED BY RATING FACTOR (RF)		
Guardrail Type L/R:	0None	/ 0	None	Inventory Rating:	1.020(36)	Load Rating Date:	10/07/2015		
Toll Facility Indicator:	0 No Toll			Operating Rating:	1.710(61)	Railroad Crossing Info			
Latitude:	41.48541377	S Longitude:	88.68699103	S	Design Load:	02 HS20	Crossing 1 Nbr:		
Deck Structure Type:	A CIP CON NRMLLY FORM			Deck Structure Thickness:	7.5	SD:	Y	FO:	N
Sidewalks Under Structure:	0 None					RR Vertical Underclear:	0	Ft	0
						In			

Key Route On Data

Key Route Nbr:	FEDERAL-AID PRIMARY	0607	Station:	6.6600
Appurtenances	Main Route	00000	Segment:	
Inventory County:	050 LASALLE		Linked:	Y
Township/Road Dist	31 SERENA		Natl. Hwy System:	Not on NHS
Municipality	0000		Inventory Direction:	
Urban Area:	None	0000	Curr AADT Yr/Count:	2015 / 2100
Functional Class:	4 MINOR ARTERIAL		Est Truck Percentage:	10
** CLEARANCES **	South/East	North/West	Number Of Lanes:	2
Max Rdwy Width:	32.0		One Or Two Way:	2 Two-Way
Horizontal:	32.0	0.0	Bypass Length:	6
			Future AADT Yr/Cnt:	2032 / 2122
			Designated Truck Rte:	CLASS II
Lateral:			Special Systems:	No

Key Route Under Data

Station:	
Segment:	
Linked:	
Natl. Hwy System:	
Inventory Direction:	
Curr AADT Yr/Count:	/
Est Truck Percentage:	
Number Of Lanes:	
One Or Two Way:	
Bypass Length:	
Future AADT Yr/Cnt:	/
Designated Truck Rte:	
Special Systems:	

***** Marked Route On Data *****

	Designation	Kind	Number
Route #1:	1 Mainline	2 U.S. Highways	052
Route #2:	1 Mainline		
Route #3:	1 Mainline		

***** Marked Route Under Data *****

Designation	Kind	Number
-------------	------	--------

Illinois Department of Transportation
Structures Information Management System
Structure Summary Report

Date: 01/04/2017

Page: 2

Structure Number: 050-0058

District: 3

Data Related to Inspection Information

*** Inspection Intervals ***

*** Maximum Allowable Posting Limits ***

Bridge Posting Level:

Routine NBIS:	12 MOS	Underwater:	0 MOS	One Truck At A Time:	0	Combination Type 3S-1:	Tons	5	No Posting Required
		Special:	N	Single Unit Vehicles:	Tons	Combination Type 3S-2:	Tons		

Inspection/Appraisal Information

Inspection Date:	08/10/2016	Inspection Temperature:	80Deg. F					** Actual Posted Limits **
Deck:	6	SATISFACTORY CONDITION - MINOR DETERIORATION					Single Unit Vehicles:	Tons
Superstructure:	4	POOR CONDITION - ADVANCED DETERIORATION					Combination Type 3S-1:	Tons
Substructure:	6	SATISFACTORY CONDITION - MINOR DETERIORATION					Combination Type 3S-2:	Tons
Culvert:	N	NOT APPLICABLE					One Truck At A Time:	0
Channel and Protection:	7	GOOD CONDITION - SOME MINOR PROBLEMS			Deck Wearing Surf:	A BARE DECK NO OVRLAY	Last Paint Type:	
Structural Evaluation:	4	MINIMUM ADEQUACY TO BE LEFT IN PLACE			Deck Membrane:	F NONE		
Deck Geometry:	4	MINIMUM ADEQUACY TO BE LEFT IN PLACE			Deck Protection:	A EPOXY COATED REINF		
Underclearance-Vert/Lat.:	N	NOT APPLICABLE			Total Deck Thick:	7.5		
Waterway Adequacy:	8	EQUAL TO PRESENT DESIRABLE CRITERIA			Last Paint Date:			
Approach Roadway Align:	8	EQUAL TO PRESENT DESIRABLE CRITERIA						
Bridge Railing Appraisal:	3	Meets Standards						
Approach Guardrail:	322	Acceptable	Not Acceptable	Not Acceptable				
Pier Navig Protection:	N	N/A						

Underwater Inspection/Appraisal Information

Inspection Date:

Temperature: Inspection Method:

Appraisal Rating:

Scour Critical Information

Miscellaneous

Rating:	8	CALCULATED SCOUR ABOVE FOOTING	Evaluation Method:	B	Rational Analysis		
Analysis Date:	11/23/1992					Microfilm Data Recorded:	Yes

Construction Information

Year:	1931	Original	1980	Reconstructed
Route:	FA-607	Sta: 353+79	FA-607	Sta: 353+79
Section Nbr:	125-B,125-BR		125-B,125-BR	
Contract Nbr:				
Fed Aid Pr#:	BR-F-607027000		BR-F-607027000	
Built By:	1	I.D.O.T.	0	UNKNOWN

Illinois Department of Transportation
Structures Information Management System
Master Structure Report (S-107)

Date: 11/28/2022

Page 1

Structure Number: 050-0058

District: 3

Inventory Data

Facility Carried:	US ROUTE 52	Bridge Name:		Sufficiency Rating:	64.3	Structure Length:	459.0
Feature Crossed:	FOX RIVER	Location:	2.10 MI W OF ILL 71	HBP Eligible:	Yes	AASHTO Bridge Length:	99.9
Bridge Remarks:				Replaced By:		Length of Long Span:	80.0
Bridge Status:	1 OPEN - NO RESTRICT	StatusDate:	4/1/1988	Replaces:		Bridge Roadway Width:	32.0
Status Remarks:				Last Update Date:	03/30/2021	Appr Roadway Width:	24.0
Maint County:	050 LASALLE	Maint Township:	31 SERENA	Parallel Structure:	None	Deck Width:	35.0
Maint Responsibility:	10 I.D.O.T.		UNKNOWN	Multi-Level Structure Nbr:		Sidewalk Width Right:	0.0
Service On/Under:	1 HIGHWAY	/	5 WATERWAY	Skew Direction:	None	Sidewalk Width Left:	0.0
Reporting Agency:	1 I.D.O.T. - BUREAU OF MAINTENANCE			Skew Angle:	0 D	Navigation Control:	0 No
Main Span Matl/Type:	1 CONCRETE	/	25 ARCH-DECK, OPEN SPANDREL	Structure Flared:	No	Navigation Horiz Clear:	0
Nbr Of Main Spans:	5	Nbr Of Approach Spans:	2	Historical Significance:	Yes	Navigation Vert Clear:	0
Approaches				Border Bridge State:		Culvert Fill Depth:	0.0
Near #1 Matl/Type:	1 CONCRETE	/	01 SLAB	Bdr State SN:		Number Culvert Cells:	0
Near #2 Matl/Type:		/		Bdr State % Responsibility:	0	Culvert Opening Area:	0.0
Far #1 Matl/Type:	1 CONCRETE	/	01 SLAB	Structural Steel Wt:	0	Culvert Cell Height:	0.00
Far #2 Matl/Type:		/		Substructure Material:		Culvert Cell Width:	0.00
Median Width/Type:	0 Ft. / 0 None			Rated By:	2 IDOT	Rate Method:	6 LOAD FACTOR (LF) REP
Guardrail Type L/R:	0 None / 0 None			Inventory Rating:	1.020 (36)	Load Rating Date:	10/07/2015
Toll Facility Indicator:	0 No Toll			Operating Rating:	1.710 (61)	***Railroad Crossing Info***	
Latitude:	41.48541377	Longitude:	88.68699103	Design Load:	02 HS20	Crossing 1 Nbr:	
Deck Structure Type:	A CIP CON NRMLLY FORM	Deck Structure Thickness:	7.5	SD:	Y	FO:	N
Sidewalks Under Structure:	0 None					RR Lateral Underclear:	0.0
						RR Vertical Underclear:	0 Ft 0 In

Key Route On Data

Key Route Nbr:	FEDERAL-AID PRIMARY	0607	Station:	6.6600
Appurtenances	Main Route	00000	Segment:	
Inventory County:	050 LASALLE		Linked:	Y
Township/Road Dist	31 SERENA		Natl. Hwy System:	Not on NHS
Municipality	0000		Inventory Direction:	
Urban Area:	None		Curr AADT Yr/Count:	2021 / 2300
Functional Class:	4 MINOR ARTERIAL		Est Truck Percentage:	11 %
** CLEARANCES **	South/East	North/West	Number Of Lanes:	2
Max Rdwy Width:	32.0		One Or Two Way:	2 Two-Way
Horizontal:	32.0	0.0	Bypass Length:	6
Min Vertical:	99Ft 11In	00Ft 00In	Future AADT Yr/Cnt:	2046 / 2323
10 Ft Vertical:	99Ft 11In	00Ft 00In	Designated Truck Rte:	CLASS II
Lateral:			Special Systems:	No

*** Marked Route On Data ***

	Designation	Kind	Number
Route #1:	1 Mainline	2 U.S. Highways	052
Route #2:	1 Mainline		
Route #3:	1 Mainline		

Key Route Under Data

		Station:	
		Segment:	
		Linked:	
		Natl. Hwy System:	
		Inventory Direction:	
		Curr AADT Yr/Count:	/
		Est Truck Percentage:	%
		Number Of Lanes:	
		One Or Two Way:	
		Bypass Length:	
		Future AADT Yr/Cnt:	/
		Designated Truck Rte:	
		Special Systems:	

*** Marked Route Under Data ***

	Designation	Kind	Number

Illinois Department of Transportation
Structures Information Management System
Master Structure Report (S-107)

Date: 11/28/2022

Page 2

Structure Number: 050-0058

District: 3

Data Related to Inspection Information

Inspection Intervals

Routine NBIS: 12 MOS Underwater: 0 MOS
Fracture Critical: 0 MOS Special: N

*** Maximum Allowable Posting Limits ***

One Truck At A Time: 0 Tons
Single Unit Vehicles: Tons
Combination Type 3S-1: Tons
Combination Type 3S-2: Tons

Bridge Posting Level:

5 No Posting Required

Inspection/Appraisal Information

Inspection Date: 08/26/2022 Inspection Temperature: 71 Deg. F
Deck: 4 POOR CONDITION - ADVANCED DETERIORATION
Superstructure: 4 POOR CONDITION - ADVANCED DETERIORATION
Substructure: 4 POOR CONDITION - ADVANCED DETERIORATION
Culvert: N NOT APPLICABLE
Channel and Protection: 7 GOOD CONDITION - SOME MINOR PROBLEMS
Structural Evaluation: 4 MINIMUM ADEQUACY TO BE LEFT IN PLACE
Deck Geometry: 4 MINIMUM ADEQUACY TO BE LEFT IN PLACE
Underclearance-Vert/Lat.: N NOT APPLICABLE
Waterway Adequacy: 8 EQUAL TO PRESENT DESIRABLE CRITERIA
Approach Roadway Align: 8 EQUAL TO PRESENT DESIRABLE CRITERIA
Bridge Railing Appraisal: 3 Meets Standards
Approach Guardrail: 222 Not Acceptable Not Acceptable Not Acceptable
Pier Navig Protection: N N/A

Insp by (Name): Czibor, Vincent L
Insp by (Name): Luis Calderon
Utilities Attached: N N/A
N N/A
N N/A
** Actual Posted Limits **
Single Unit Vehicles: Tons
Combination Type 3S-1: Tons
Combination Type 3S-2: Tons
One Truck At A Time: 0

Deck Wearing Surf: A BARE DECK NO OVRLAY
Deck Membrane: F NONE
Deck Protection: A EPOXY COATED REINF
Total Deck Thick: 7.5
Last Paint Date: Last Paint Type:

Inspection Remarks:

2022 Deck surface and soffit have more cracking, delamination's and spalling. L arge spall in westbound lane filled patchily with cold mix (span 3 from east). Arches have large areas of cracking, delamination's and spalls with exposed cor

Underwater Inspection/Appraisal Information

Inspection Date: Temperature: Inspection Method: Inspected By: Inspected By: Appraisal Rating: Inspection Remarks:

Scour Critical Information

Rating: 8 CALCULATED SCOUR ABOVE FOOTING Evaluation Method: B Rational Analysis
Analysis Date: 11/23/1992 Analysis By: MCCARTER

Miscellaneous

Fracture Critical Members: No
Microfilm Data Recorded: Yes

Construction Information

Year: 1931 Original 1980 Reconstructed
Route: FA-607 Sta: 353+79 FA-607 Sta: 353+79
Section Nbr: 125-B,125-BR 125-B,125-BR
Contract Nbr:
Fed Aid Pr #: BR-F-607027000 BR-F-607027000
Built By: 1 I.D.O.T. 0 UNKNOWN

Proposed Improvement

Cost Estimate Year: Length: *** Costs in Dollars ***
Type of Work: Bridge Cost:
Done By: Roadway Cost:
Remarks: Total Project Cost:

Illinois Department of Transportation
Structures Information Management System
Master Structure Report (S-107)

Date: 10/4/2024

Page 1

Structure Number: 050-0058

District: 3

Inventory Data

Facility Carried:	US 52	Bridge Name:		Sufficiency Rating:	33.6	Structure Length:	459.0					
Feature Crossed:	FOX RIVER	Location:	2.10 MI W OF ILL 71	HBP Eligible:	Yes	AASHTO Bridge Length:	99.9					
Bridge Remarks:				Replaced By:		Length of Long Span:	80.0					
Bridge Status:	2	OPEN - LOAD POSTED	StatusDate:	2/2/2023	Replaces:	050-0266	Bridge Roadway Width:	32.0				
Status Remarks:				Last Update Date:	09/10/2024	Appr Roadway Width:	24.0					
Maint County:	050	LASALLE	Maint Township:	31	SERENA	Parallel Structure:	None	Deck Width:	35.0			
Maint Responsibility:	10	I.D.O.T.	UNKNOWN		Multi-Level Structure Nbr:		Sidewalk Width Right:	0.0				
Service On/Under:	1	HIGHWAY	/	5	WATERWAY	Skew Direction:	None	Sidewalk Width Left:	0.0			
Reporting Agency:	1	I.D.O.T. - BUREAU OF MAINTENANCE			Skew Angle:	0	D	Navigation Control:	0	No		
Main Span Matl/Type:	1	CONCRETE	/	25	ARCH-DECK, OPEN SPANDREL	Structure Flared:	No	Navigation Horiz Clear:	0			
Nbr Of Main Spans:	5	Nbr Of Approach Spans:	2	Historical Significance:		Yes	Navigation Vert Clear:	0				
Approaches				Border Bridge State:			Culvert Fill Depth:	0.0				
Near #1 Matl/Type:	1	CONCRETE	/	01	SLAB	Bdr State SN:		Number Culvert Cells:	0			
Near #2 Matl/Type:			/			Bdr State % Responsibility:	0	Culvert Opening Area:	0.0			
Far #1 Matl/Type:	1	CONCRETE	/	01	SLAB	Structural Steel Wt:	0	Culvert Cell Height:	0.00			
Far #2 Matl/Type:			/			Substructure Material:	55	Culvert Cell Width:	0.00			
Median Width/Type:	0	Ft. /	0	None	Rated By:	3	Consultant	Rate Method:	6	LOAD FACTOR (LF) REP		
Guardrail Type L/R:	0	None	/	0	None	Inventory Rating:	0.330	(11)	Load Rating Date:	01/27/2023		
Toll Facility Indicator:	0	No Toll	Operating Rating:		1.030	(37)	***Railroad Crossing Info***					
Latitude:	41.48541377	Longitude:	88.68699103	Design Load:		02	HS20	Crossing 1 Nbr:				
Deck Structure Type:	A	CIP CON NRMLLY FORM	Deck Structure Thickness:		7.5	SD:	Y	FO:	Y	Crossing 1 Nbr:		
Sidewalks Under Structure:	0	None						RR Lateral Underclear:	0.0			
								RR Vertical Underclear:	0	Ft	0	In

Key Route On Data

Key Route Nbr:	FEDERAL-AID PRIMARY	0607	Station:	6.6600		
Appurtenances	Main Route	00000	Segment:			
Inventory County:	050	LASALLE	Linked:	Y		
Township/Road Dist	31	SERENA	Natl. Hwy System:	Not on NHS		
Municipality:	0000		Inventory Direction:			
Urban Area:	None		Curr AADT Yr/Count:	2023 / 1850		
Functional Class:	4	MINOR ARTERIAL	Est Truck Percentage:	12 %		
** CLEARANCES **	South/East	North/West	Number Of Lanes:	2		
Max Rdwy Width:	32.0		One Or Two Way:	2	Two-Way	
Horizontal:	32.0	0.0	Bypass Length:	6		
Min Vertical:	99Ft	11In	00Ft	00In	Future AADT Yr/Cnt:	2046 / 2323
10 Ft Vertical:	99Ft	11In	00Ft	00In	Designated Truck Rte:	CLASS II
Lateral:			Special Systems:	No		

*** Marked Route On Data ***

	Designation	Kind	Number
Route #1:	1 Mainline	2 U.S. Highways	052
Route #2:	1 Mainline		
Route #3:	1 Mainline		

Key Route Under Data

	Station:	
	Segment:	
	Linked:	
	Natl. Hwy System:	
	Inventory Direction:	
	Curr AADT Yr/Count:	/
	Est Truck Percentage:	%
	Number Of Lanes:	
	One Or Two Way:	
	Bypass Length:	
	Future AADT Yr/Cnt:	/
	Designated Truck Rte:	
	Special Systems:	

*** Marked Route Under Data ***

	Designation	Kind	Number

Illinois Department of Transportation
Structures Information Management System
Master Structure Report (S-107)

Date: 10/4/2024

Page 2

Structure Number: 050-0058

District: 3

Data Related to Inspection Information

Inspection Intervals

Routine NBIS: 12 MOS Underwater: 0 MOS
Fracture Critical: 0 MOS Special: N

*** Maximum Allowable Posting Limits ***

One Truck At A Time: 0 Tons
Single Unit Vehicles: 23 Tons
Combination Type 3S-1: 39 Tons
Combination Type 3S-2: 39 Tons

Bridge Posting Level:

0 >= 40% Below Legal Loads

Inspection/Appraisal Information

Inspection Date: 08/12/2024 Inspection Temperature: 69 Deg. F
Deck: 4 POOR CONDITION - ADVANCED DETERIORATION
Superstructure: 4 POOR CONDITION - ADVANCED DETERIORATION
Substructure: 4 POOR CONDITION - ADVANCED DETERIORATION
Culvert: N NOT APPLICABLE
Channel and Protection: 7 GOOD CONDITION - SOME MINOR PROBLEMS
Structural Evaluation: 3 INTOLERABLE - HIGH PRIORITY FOR CORRECTION
Deck Geometry: 5 BETTER THAN ADEQUATE TO BE LEFT IN PLACE
Underclearance-Vert/Lat.: N NOT APPLICABLE
Waterway Adequacy: 8 EQUAL TO PRESENT DESIRABLE CRITERIA
Approach Roadway Align: 8 EQUAL TO PRESENT DESIRABLE CRITERIA
Bridge Railing Appraisal: 3 Meets Standards
Approach Guardrail: 222 Not Acceptable Not Acceptable Not Acceptable
Pier Navig Protection: N N/A

Insp by (Name): Calderon, Luis E

Insp by (Name): Luis calderon

Utilities Attached:

Deck Wearing Surf: A BARE DECK NO OVRLAY

Deck Membrane: F NONE

Deck Protection: A EPOXY COATED REINF

Total Deck Thick: 7.5

Last Paint Date:

Inspection Remarks:

24' All elements appear to have slightly deteriorated. No significant changes f rom 2023 insp. Load postings in place. All joints have significant damage / sec loss. where they are no longer effective. Moderate settlement @ E APPR (above

** Actual Posted Limits **

Single Unit Vehicles: 23 Tons

Combination Type 3S-1: 39 Tons

Combination Type 3S-2: 39 Tons

One Truck At A Time: 0

Last Paint Type:

Underwater Inspection/Appraisal Information

Inspection Date: Temperature: Inspection Method: Inspected By: Inspected By: Appraisal Rating: Inspection Remarks:

Scour Critical Information

Rating: 8 CALCULATED SCOUR ABOVE FOOTING Evaluation Method: B Rational Analysis
Analysis Date: 11/23/1992 Analysis By: MCCARTER

Miscellaneous

Fracture Critical Members: No
Microfilm Data Recorded: Yes

Construction Information

Year: 1931 Original 1980 Reconstructed
Route: FA-607 Sta: 353+79 FA-607 Sta: 353+79
Section Nbr: 125-B,125-BR 125-B,125-BR
Contract Nbr:
Fed Aid Pr #: BR-F-607027000 BR-F-607027000
Built By: 1 I.D.O.T. 0 UNKNOWN

Proposed Improvement

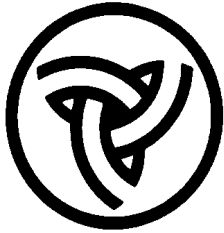
Cost Estimate Year: Length: Type of Work: Done By: Remarks:

*** Costs in Dollars ***

Bridge Cost: Roadway Cost: Total Project Cost:

APPENDIX C

CORRESPONDENCES & DOCUMENTATIONS



Illinois Department of Transportation

Memorandum

To:	Kevin Marchek	Attn:	Roger Rynke
From:	Maureen Addis	By:	Brad Koldehoff
Subject:	Historic Bridge Coordination		
Date:	September 27, 2016		

LaSalle County
Norway
FAP 607 (US 52)
Bridge over Fox River
Structure # 050-0058
Job # P-93-015-17
IDOT Sequence # 20206

We have received an Environmental Survey Request for the above-referenced project involving a Concrete Arch Deck - Open Spandrel bridge (S.N. 050-00548), which is included on the Illinois Historical Bridge List as an example of this bridge type. This bridge is a rare bridge type – only fourteen concrete arch deck open spandrel bridges still stand in the state. As such, this bridge is considered eligible for listing on the National Register of Historic Places, and therefore, is accorded protection under Section 106 of the National Historic Preservation Act of 1966, as amended (36 CFR 800).

Based on the submitted information, the current plans are to replace this bridge; however, its replacement would constitute an adverse effect. FHWA policy requires that all reasonable measures be taken to avoid the demolition of this bridge. **Rehabilitation of the existing structure must be considered.** If rehabilitation is not feasible, an attempt must be made to avoid the structure by construction of the replacement bridge on a new alignment. If there is no feasible or prudent alternative to demolition, a Section 106/4(f) report will be required in order to begin coordination with the Illinois State Historic Preservation Officer (SHPO).

Please submit information regarding on the chosen course of action (i.e. plans of the repairs/rehabilitation, new alignment, or the Section 106/4(f) report) to our office in order to initiate SHPO consultation.

Brad H. Koldehoff, RPA
Cultural Resources Unit
Bureau of Design and Environment
BK:el



Illinois Department of Transportation

Memorandum

To: Trisha Thompson Attn: Roger Rynke
From: Jack Elston By: Joseph Galloy
Subject: Adverse Effect – SHPO Coordination Required
Date: June 20, 2024

LaSalle County
Serena, Sheridan, Norway
FAP 607 / US 52
Bridge Replacement
Structure # 050-0058
Contract # 66F85
IDOT Sequence # 20206
SHPO Log # 003041924

Further coordination with the Illinois State Historic Preservation Officer (SHPO) is required for the above referenced undertaking. Attached is a letter from the SHPO indicating that the undertaking as currently planned will constitute an Adverse Effect pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended, as it will adversely affect Structure # 050-0058, which is a historic bridge that is eligible for listing on the National Register of Historic Places (NRHP).

As a next step, we have received the Section 4(f) report and comments will be returned to your office shortly. IDOT Cultural Resources Unit will also develop a draft memorandum of agreement (MOA) that will contain mitigation that must be completed before any construction can be initiated.

A handwritten signature in cursive script, reading "Joseph M. Galloy".

Joseph M. Galloy, PhD, RPA
Cultural Resources Unit Manager
Bureau of Design & Environment

JG:el
Attachment



Illinois
Department of
**Natural
Resources**

JB Pritzker, Governor • Natalie Phelps Finnie, Director
One Natural Resources Way • Springfield, Illinois 62702-1271

www.dnr.illinois.gov

LaSalle County

Serena, Sheridan, Norway

Bridge Replacement, FAP 607/U.S. 52 over the Fox River (SN 050-0058)

IDOT-P-93-015-17, IDOT Seq #-20206

SHPO Log #003041924

May 16, 2024

Joseph Galloy

Illinois Department of Transportation

Bureau of Design and Environment

2300 S. Dirksen Parkway

Springfield, IL 62764

Thank you for your submission of the proposed bridge replacement project in LaSalle County, Illinois, which we received on 4/19/24 (SHPO Log# 003041924). Our comments are required by Section 106 of the [National Historic Preservation Act of 1966](#), as amended, 54 U.S.C. § 306108, and its [implementing regulations](#) (36 CFR Part 800) (Act).

We concur with the established Area of Potential Effects (APE) and the identification of historic properties within the APE:

- FAP 607/US 52 over Fox River, Sheridan (SN 050-0058). Previously determined eligible for listing on the National Register of Historic Places (NRHP), as an alternate on IDOT's Historic Bridge List.

Additionally, our staff have determined that no historic archaeological properties are known to exist within the APE. However, if any archaeological materials are encountered during construction, this office must be notified. This letter is not a clearance for purposes of the [Illinois Human Remains Protection Act \(20 ILCS 3440\)](#).

Lastly, we concur with the determination that the proposed project will adversely affect the historic bridge (SN 050-0058). Please work with CJ Wallace (Carol.Wallace@Illinois.gov) in our office to avoid, minimize, or mitigate the adverse effects pursuant to the Act.

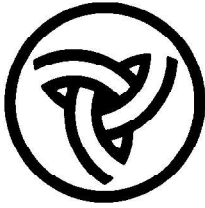
Sincerely,

Carey L. Mayer, AIA

Deputy State Historic Preservation Officer

APPENDIX D

PUBLIC INVOLVEMENT & PROJECT COORDINATION



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Kitty Henderson
Historic Bridge Foundation
P.O. Box 66245
Austin, TX 78766

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Ms. Henderson:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

The project is tentatively scheduled in the department's *Fiscal Year 2023 – 2028, Proposed Highway Improvement Program*, subject to project readiness and funding availability.

SN 050-0058 is eligible for listing in the National Register of Historic Places (NRHP) as a Concrete Arch Deck – Open Spandrel bridge and is therefore accorded protection under Section 106 of the National Historic Preservation Act as well as Section 4(f) of the National Environmental Policy Act. It is the policy of the department to provide consulting parties who have been identified as an agency or organization that may have interest in this project the opportunity to comment on the project's potential effects on historic resources.

Please provide any comments by June 5, that your agency or organization consider pertinent to our early study phase so that comments can be reviewed and incorporated into the report being compiled to summarize the project, evaluate alternatives, and document necessary consultation.

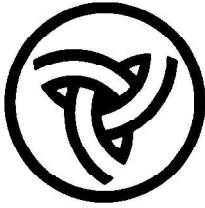
Kitty Henderson
Historic Bridge Foundation
May 11, 2023
Page 2

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.
Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Nathan Holth
HistoricBridges.org
2767 Eastway Drive
Okemoa, MI 49864

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Mr. Holth:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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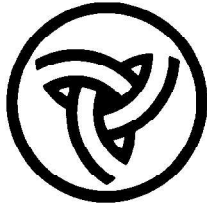
Nathan Holth
HistoricBridges.org
May 11, 2023
Page 2

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.
Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Lisa DiChiera
Landmarks Illinois
30 N. Michigan Ave. Suite 2020
Chicago, IL 60602

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Ms. DiChiera:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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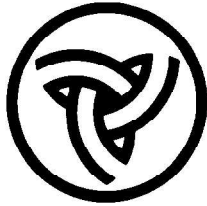
Lisa DiChiera
Landmarks Illinois
May 11, 2023
Page 2

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.
Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

LaSalle County Historical Society
101 E. Canal St.
North Utica, IL 61373

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Sir or Madam:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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Please provide any comments by June 5, that your agency or organization consider pertinent to our early study phase so that comments can be reviewed and incorporated into the report being compiled to summarize the project, evaluate alternatives, and document necessary consultation.

LaSalle County Historical Society
May 11, 2023
Page 2

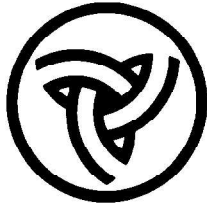
If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.

Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Sheridan Historical Society
P.O. Box 580
Sheridan, IL 60551

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Sir or Madam:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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Please provide any comments by June 5, that your agency or organization consider pertinent to our early study phase so that comments can be reviewed and incorporated into the report being compiled to summarize the project, evaluate alternatives, and document necessary consultation.

Sheridan Historical Society
May 11, 2023
Page 2

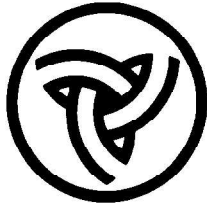
If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.

Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Central Illinois Landmarks Foundation
P.O. Box 495
Peoria, IL 61651

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Sir or Madam:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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Central Illinois Landmarks Foundation
May 11, 2023
Page 2

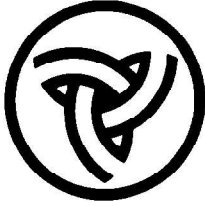
If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.

Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

May 11, 2023

Bill Kenney
Northern Illinois Historic League
1037 Boxwood Dr.
Crystal Lake, IL 60014

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Mr. Kenney:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the roadway improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

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Bill Kenney
Northern Illinois Historic League
May 11, 2023
Page 2

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Acting Location and Environmental Studies Engineer, at 815-434-8469.
Sincerely,

Masood Ahmad, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer

66F85 – US 52 Fox River Bridge
Response Summary

Parties	Contact Information	Comment(s)
Historic Bridge Foundation	Attn: Kitty Henderson, P.O. Box 66245, Austin, TX 78766, kitty@historicbridgefoundation.com	Letter was returned to District 3 office. An email with a copy of the letter was sent to Historic Bridge Foundation on 01/04/2024. A reply email was received on the same day. See attachment.
HistoricBridges.org	Attn: Nathan Holth, webmaster@historicbridges.org	No comments were received to date.
Landmarks Illinois	Attn: Quinn Adamowski, 30 N. Michigan Ave., Suite 2020, Chicago, IL 60602-3402, qadamowski@landmarks.org	Comments received. See attachment. The district was able to provide the project website containing bridge photos.
LaSalle County Historical Society	101 E. Canal St., North Utica, IL 61373, office@lasallecountyhistoricalsociety.org	No comments were received to date.
Sheridan Historical Society	185 N. Robinson St., Sheridan, IL 60551	No comments were received to date.
Central Illinois Landmarks Foundation	416 Hamilton Blvd., Peoria, IL 61602, cilf.peoria@gmail.com	No comments were received to date.
Northern Illinois Historic League	Attn: Bill Kenney, 1037 Boxwood Dr., Crystal Lake, IL 60014, lhgdr@gmail.com	Comments received. See attachment.

Banga, Kenneth T.

From: Kitty Henderson <kitty@historicbridgefoundation.com>
Sent: Thursday, January 4, 2024 12:06 PM
To: Banga, Kenneth T.
Subject: [External] Re: US Route 52 Historic Bridge over the Fox River in LaSalle County, IL (IDOT project)

Follow Up Flag: Follow up
Flag Status: Flagged

Thank you for contacting me. I don't have any questions about the project. The Historic Bridge Foundation does not wish to be a consulting party on the project.

Kitty Henderson
Historic Bridge Foundation
1500 Payne Ave
Austin, Texas 78757
512-585-1814

On Jan 4, 2024, at 10:28 AM, Banga, Kenneth T. <Kenneth.Banga@illinois.gov> wrote:

Good morning, Kitty.

I am reaching out to you because my records show that the attached letter addressed to Historic Bridge Foundation was returned to the department (IDOT) last year.

I wanted to make sure that you receive this information to see if you have any questions or comments about this upcoming transportation project.

Please feel free to reach out at any time.

Thank you,

Kenneth Banga, P.E.
Acting Location and Environmental Studies Engineer
Program Development / Studies and Plans
IDOT / District 3
815-434-8469 (Office)
Kenneth.Banga@illinois.gov

State of Illinois - CONFIDENTIALITY NOTICE: The information contained in this communication is confidential, may be attorney-client privileged or attorney work product, may constitute inside information or internal deliberative staff communication, and is intended only for the use of the addressee. Unauthorized use, disclosure or copying of this communication or any part thereof is strictly prohibited and may be unlawful. If you have received this communication in error, please notify the

sender immediately by return e-mail and destroy this communication and all copies thereof, including all attachments. Receipt by an unintended recipient does not waive attorney-client privilege, attorney work product privilege, or any other exemption from disclosure.

<2023-5-15 Returned Mail - Historic Bridge Foundation - 66F85.pdf>

Banga, Kenneth T.

From: Banga, Kenneth T.
Sent: Thursday, January 4, 2024 4:03 PM
To: qadamowski@landmarks.org
Subject: US Route 52 Bridge Over Fox River
Attachments: US 52 Bridge Fox River_LaSalle Co.pdf

Good afternoon, Mr. Adamowski,

I am reaching out to you about the status of this IDOT project. In the attached response sheet we received, you inquired to see photos of the bridge.

A website is available at [LaSalle County: U.S. 52 Fox River Bridge Replacement Study \(illinois.gov\)](https://www.illinois.gov/transportation/infrastructure/bridges/la-salle-county-us-52-fox-river-bridge-replacement-study) which contains additional information about the project including bridge photos under Resources.

Please let me know if you have additional questions.

Thank you,

Ken Banga

From: Quinn Adamowski <qadamowski@landmarks.org>
Sent: Tuesday, June 6, 2023 1:34 PM
To: Banga, Kenneth T. <Kenneth.Banga@illinois.gov>
Subject: [External] US Route 52 Bridge Over Fox River

Hi Mr. Banga,

Please see the attached form in response to the letter from May 11, 2023 that references a proposed project to replace the bridge carrying US 52 over the Fox River, located 2.1 miles west of IL Route 71 in LaSalle County.

Please let me know if you have any questions.

Thank You,

Quinn Adamowski
Regional Advocacy Manager
[Landmarks Illinois](https://www.landmarksillinois.org/)

30 N. Michigan Avenue, Suite 2020, Chicago, IL 60602
O: 815-603-4901 [Landmarks.org](https://www.landmarksillinois.org/) [Facebook](https://www.facebook.com/landmarksillinois/) [Twitter](https://twitter.com/landmarksillinois/) [Instagram](https://www.instagram.com/landmarksillinois/)
People Saving Places for People. [Join us today](#)

[Support People Saving Places! Join Landmarks Illinois' Monthly Giving Membership Program!](#)

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85.

**PLEASE VERIFY THE FOLLOWING
CONTACT INFORMATION:**

☐ CORRECT

☒ **INCORRECT**
(Please print changes below)

Landmarks Illinois
Lisa DiChiera
30 N. Michigan Ave. Suite 2020
Chicago, IL 60602
(312) 922-1742, ext. 229

CHECK THE APPROPRIATE RESPONSE:

☐ I have no comments at this time.

☒ I have noted my comments below.

☐ I would like to discuss this matter further by telephone.

☐ I will call you

☐ Please call me at _____ Preferred date and time: _____
(phone no.)

☐ I would like to personally meet to discuss this project.

Please call me to arrange a specific date, time and location.

I can be reached at (Phone #): _____

The most convenient time to contact me is (day and time) _____

COMMENTS

I am not familiar with this bridge. I reached out to SHPO to get some background, but they do not have anything on file as consultation has not commenced. Could you please send me photos of the bridge?

I reserve comment until SHPO has a chance to review and provides finding of effect.

NAME:

Quinn Adamowski

Please print

SIGNATURE:



Landmarks Illinois
Quinn Adamowski
30 N Michigan Ave Suite2020
Chicago, IL 60602
815-603-4901

DATE:

6/5/23

KB

Banga, Kenneth T.

From: Banga, Kenneth T.
Sent: Wednesday, June 28, 2023 4:00 PM
To: Illinois Historic Group Directory project - NIHL
Cc: Ahmad, Masood; Alexander, David S
Subject: RE: [External] LASALLE COUNTY - US 52 BRIDGE OVER FOX RIVER - SN 050-0058 - NATIONAL REGISTER IMPLICATIONS - YOUR MAY 11 LETTER TO THE NIHL OFFICE

Good afternoon, Mr. Kenney

Thank you for your response.

The Department will continue to include you as a Consulting Party for this project. Also, we are coordinating this project with our Cultural Resources Unit (CRU) in Springfield. CRU will initiate consultation with the SHPO and seek concurrence from the SHPO on the adverse effect to the historic bridge and potential mitigation measures.

Best Regards,

Kenneth Banga, P.E.
Acting Location and Environmental Studies Engineer
Program Development / Studies and Plans
IDOT / District 3
815-434-8469 (Office)
Kenneth.Banga@illinois.gov

From: Illinois Historic Group Directory project - NIHL <ihgdir@gmail.com>
Sent: Tuesday, June 13, 2023 4:15 PM
To: Ahmad, Masood <Masood.Ahmad@illinois.gov>
Cc: Banga, Kenneth T. <Kenneth.Banga@illinois.gov>; Alexander, David S <David.S.Alexander@illinois.gov>; Heckenkamp, Andrew <Andrew.Heckenkamp@Illinois.gov>; mkindelin@jlkarch.com
Subject: [External] LASALLE COUNTY - US 52 BRIDGE OVER FOX RIVER - SN 050-0058 - NATIONAL REGISTER IMPLICATIONS - YOUR MAY 11 LETTER TO THE NIHL OFFICE

Hello Masood,

Please contact Andrew Heckenkamp at the Illinois State Historic Preservation Office (SHPO), if not already contacted, about your project for the US 52 Bridge replacement over the Fox River since you are reporting the existing structure is eligible for listing in the National Register of Historic Places.

The Northern Illinois Historic League has poor subject matter experts at the local level currently from area volunteers in LaSalle County. I might need IDOT to designate a subject matter expert such as yourself to prepare a nomination package for the Illinois Historic Sites Advisory Council (IHSAC) to be considered at a future triannual meeting where National Register nominations are considered. I have also copied Meg Kindelin, IHSAC current chair, on this followup to you. I have also copied Kenneth and David since you copied them with the original letter to my office.

You may continue to include me as a Section 106 Consulting Party for this project, but I would likely not be participating more directly than that since this is a local LaSalle County IDOT project.

Bill Kenney
Illinois Historic Group Directory Administrator
(815) 649-1700



Illinois Historic Group Directory



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

July 9, 2024

Landmarks Illinois
Attn: Quinn Adamowski
30 N. Michigan Ave., Ste. 2020
Chicago, IL 60602-3402

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Mr. Adamowski:

The purpose of this letter is to provide an update to the initial consultation letters dated May 11, 2023, regarding a spot improvement on US Route 52 located about 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

The project is tentatively scheduled in the department's recently published *Fiscal Year 2025 – 2030, Proposed Highway Improvement Program*, subject to project readiness and funding availability.

The bridge (SN 050-0058) is eligible for listing in the National Register of Historic Places (NRHP) as a Concrete Arch Deck – Open Spandrel bridge and is therefore accorded protection under Section 106 of the National Historic Preservation Act as well as Section 4(f) of the National Environmental Policy Act. As part of the department's coordination efforts, your agency or organization has been identified as a potential consulting party who may have interest in this project. The SHPO concurred with IDOT's historic properties identification and the adverse effect determination on May 16, 2024. See attached IDNR letter for more details.

Please provide any additional comments by **August 1, 2024**, that your agency or organization consider pertinent to our early study phase so that these comments can be incorporated into the project report as well as document the necessary consultation.

Landmarks Illinois
Page 2
July 9, 2024

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Location and Environmental Studies Engineer, at (815) 434-8469.

Sincerely,

Trisha Thompson, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

July 9, 2024

Northern Illinois Historic League
Attn: Bill Kenney
1037 Boxwood Dr.
Crystal Lake, IL 60014

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Mr. Kenney:

The purpose of this letter is to provide an update to the initial consultation letters dated May 11, 2023, regarding a spot improvement on US Route 52 located about 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

The project is tentatively scheduled in the department's recently published *Fiscal Year 2025 – 2030, Proposed Highway Improvement Program*, subject to project readiness and funding availability.

The bridge (SN 050-0058) is eligible for listing in the National Register of Historic Places (NRHP) as a Concrete Arch Deck – Open Spandrel bridge and is therefore accorded protection under Section 106 of the National Historic Preservation Act as well as Section 4(f) of the National Environmental Policy Act. As part of the department's coordination efforts, your agency or organization has been identified as a potential consulting party who may have interest in this project. The SHPO concurred with IDOT's historic properties identification and the adverse effect determination on May 16, 2024. See attached IDNR letter for more details.

Please provide any additional comments by **August 1, 2024**, that your agency or organization consider pertinent to our early study phase so that these comments can be incorporated into the project report as well as document the necessary consultation.

Northern Illinois Historic League
Page 2
July 9, 2024

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Location and Environmental Studies Engineer, at (815) 434-8469.

Sincerely,

Trisha Thompson, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer

66F85 – US 52 Fox River Bridge
Follow-Up Response Summary

Parties	Contact Information	Comment(s)
Landmarks Illinois	Attn: Quinn Adamowski, 30 N. Michigan Ave., Suite 2020, Chicago, IL 60602-3402, qadamowski@landmarks.org	No additional comments received to date.
Northern Illinois Historic League	Attn: Bill Kenney, 1037 Boxwood Dr., Crystal Lake, IL 60014, lhgdr@gmail.com	No additional comments received to date.

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KENNETH BANGA Times Ord: 1 Times Run: ***
700 E NORRIS DRIVE NSTD 4.00 X 6.15 Words: 345
OTTAWA IL 61350 Total NSTD 24.60
Class: N0011 LEGALS
Rate: LEGAL Cost: 402.45
Affidavits: 1
Ad Descrpt: US 52 FOX RIVER BRIDGE
Descr Cont: 2103781
Given by: BANGA, KENNETH T.
P.O. #: _____
Contact: KENNETH BANGA Created: kateh 08/24/23 08:54
Phone: (815)434-8469 Last Changed: kateh 08/24/23 09:20
Fax#: _____
Email: Kenneth.Banga@illinois.gov
Agency: _____

URL: _____

Source: 2103771 Section: _____ Page: ____
Camera Ready: N Group: LEGALS AdType: _____
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Proof: _____ Pickup Date: _____ Ad#: 2103771
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AUTHORIZATION

Under this agreement rates are subject to change with 30 days notice. In the event of a cancellation before schedule completion, I understand that the rate charged will be based upon the rate for the number of insertions used.

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Ad #: 2103781

Status: New

NOTICE

SEEKING PUBLIC COMMENTS

**ON REPLACEMENT OF A BRIDGE CARRYING US ROUTE 52 OVER THE FOX RIVER
LOCATED 2.1 MILES WEST OF IL ROUTE 71 IN LASALLE COUNTY**

THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT), DISTRICT 3 OFFICE, IS SEEKING PUBLIC COMMENTS ON REPLACEMENT OF A BRIDGE CARRYING US ROUTE 52 OVER THE FOX RIVER LOCATED 2.1 MILES WEST OF IL ROUTE 71 IN LASALLE COUNTY. THE PURPOSE OF THIS PROJECT IS TO IMPROVE THE EXISTING US 52 BRIDGE CROSSING OVER THE FOX RIVER WHILE MINIMIZING IMPACTS TO THE TRAVELING PUBLIC AND ITS SURROUNDING ENVIRONMENT.

THE BRIDGE (SN 050-0058) IS OWNED AND MAINTAINED BY IDOT. IT IS ELIGIBLE FOR LISTING ON THE NATIONAL REGISTER OF HISTORIC PLACES (NRHP) AND THEREFORE ACCORDED PROTECTION UNDER SECTION 106 OF THE NATIONAL HISTORIC PRESERVATION ACT (NHPA) AND SECTION 4(F) OF THE US DEPARTMENT OF TRANSPORTATION ACT OF 1966. THE PROPOSED IMPROVEMENT WILL REQUIRE DEMOLITION AND REMOVAL OF THE HISTORIC BRIDGE WHICH CONSTITUTES A SECTION 4(F) USE. IDOT INTENDS TO COORDINATE THE ADVERSE EFFECT WITH THE FEDERAL HIGHWAY ADMINISTRATION (FHWA), THE STATE HISTORIC PRESERVATION OFFICER (SHPO), AND THE ADVISORY COUNCIL ON HISTORIC PRESERVATION (ACHP) THROUGH PROCEDURES PURSUANT TO SECTION 106 OF NHPA, AS AMENDED (36 CFR 800).

IDOT IS PREPARING THE ENVIRONMENTAL DOCUMENTATION SUPPORTING A CATEGORICAL EXCLUSION (CE) FOR THE PROJECT IN ACCORDANCE WITH THE NATIONAL ENVIRONMENTAL POLICY ACT OF 1969.

ALL INTERESTED PARTIES ARE INVITED TO VIEW THE PROJECT WEBSITE (<https://idot.illinois.gov/transportation-system/transportation-management/featured-projects/u-s-52-in-lasalle-county--fox-river-bridge.html>) AND PROVIDE COMMENTS. MAPS, EXISTING PLANS, PRELIMINARY DRAWINGS, AERIAL EXHIBITS, AND OTHER PERTINENT INFORMATION ARE AVAILABLE IN THE PROJECT WEBSITE FOR REVIEW.

WRITTEN COMMENTS MAY BE SUBMITTED AT THE IDOT OFFICE, MAILED TO THE IDOT OFFICE, OR SUBMITTED ELECTRONICALLY TO KENNETH.BANGA@ILLINOIS.GOV. THE PROJECT TEAM WILL RESPOND TO ALL COMMENTS RECEIVED. COMMENTS MUST BE RECEIVED BY SEPTEMBER 27, 2023, TO BE CONSIDERED AS PART OF THE PUBLIC RECORD.

ALL CORRESPONDENCE REGARDING THIS PROJECT SHOULD BE SENT TO:

IDOT DISTRICT 3
ATTENTION MR. KENNETH BANGA
700 EAST NORRIS DRIVE
OTTAWA, IL 61350

(FAX (815) 434-8553) OR (PHONE (815) 434-8469) (TTY (800) 526-0844).

(Published in the News-Tribune Aug. 28, 2023) 2103781

SHAW MEDIA
EST. 1851
PO BOX 250
CRYSTAL LAKE IL 60039-0250
(815)459-4040

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Acct #: 10217609

Ad #: 2106792

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OTTAWA IL 61350

Start: 09/07/2023 Stop: 09/07/2023

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Total CLEGO 100.00

Class: 08100 PUBLIC NOTICES

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Ad Descrpt: US 52 FOX RIVER BRIDGE

Descr Cont: 2106792

Given by: BANGA, KENNETH T.

P.O. #:

Created: kateh 09/06/23 10:55

Last Changed: kateh 09/06/23 11:01

Contact:

Phone: (815)434-8469

Fax#:

Email:

Agency:

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Group: LEGALS AdType: _____

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Printed at 09/06/23 11:01 by kateh-sm

Acct #: 10217609

Ad #: 2106792

Status: New

NOTICE

**SEEKING PUBLIC COMMENTS ON REPLACEMENT OF A BRIDGE
CARRYING US ROUTE 52 OVER THE FOX RIVER LOCATED 2.1 MILES
WEST OF IL ROUTE 71 IN LASALLE COUNTY**

THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT), DISTRICT 3 OFFICE, IS SEEKING PUBLIC COMMENTS ON REPLACEMENT OF A BRIDGE CARRYING US ROUTE 52 OVER THE FOX RIVER LOCATED 2.1 MILES WEST OF IL ROUTE 71 IN LASALLE COUNTY. THE PURPOSE OF THIS PROJECT IS TO IMPROVE THE EXISTING US 52 BRIDGE CROSSING OVER THE FOX RIVER WHILE MINIMIZING IMPACTS TO THE TRAVELING PUBLIC AND ITS SURROUNDING ENVIRONMENT.

THE BRIDGE (SN 050-0058) IS OWNED AND MAINTAINED BY IDOT. IT IS ELIGIBLE FOR LISTING ON THE NATIONAL REGISTER OF HISTORIC PLACES (NRHP) AND THEREFORE ACCORDED PROTECTION UNDER SECTION 106 OF THE NATIONAL HISTORIC PRESERVATION ACT (NHPA) AND SECTION 4(F) OF THE US DEPARTMENT OF TRANSPORTATION ACT OF 1966. THE PROPOSED IMPROVEMENT WILL REQUIRE DEMOLITION AND REMOVAL OF THE HISTORIC BRIDGE WHICH CONSTITUTES A SECTION 4(F) USE. IDOT INTENDS TO COORDINATE THE ADVERSE EFFECT WITH THE FEDERAL HIGHWAY ADMINISTRATION (FHWA), THE STATE HISTORIC PRESERVATION OFFICER (SHPO), AND THE ADVISORY COUNCIL ON HISTORIC PRESERVATION (ACHP) THROUGH PROCEDURES PURSUANT TO SECTION 106 OF NHPA, AS AMENDED (36 CFR 800).

IDOT IS PREPARING THE ENVIRONMENTAL DOCUMENTATION SUPPORTING A CATEGORICAL EXCLUSION (CE) FOR THE PROJECT IN ACCORDANCE WITH THE NATIONAL ENVIRONMENTAL POLICY ACT OF 1969.

ALL INTERESTED PARTIES ARE INVITED TO VIEW THE PROJECT WEBSITE (<https://idot.illinois.gov/transportation-system/transportation-management/featured-projects/u-s--52-in-lasalle-county--fox-river-bridge.html>) AND PROVIDE COMMENTS. MAPS, EXISTING PLANS, PRELIMINARY DRAWINGS, AERIAL EXHIBITS, AND OTHER PERTINENT INFORMATION ARE AVAILABLE IN THE PROJECT WEBSITE FOR REVIEW.

WRITTEN COMMENTS MAY BE SUBMITTED AT THE IDOT OFFICE, MAILED TO THE IDOT OFFICE, OR SUBMITTED ELECTRONICALLY TO KENNETH.BANGA@ILLINOIS.GOV. THE PROJECT TEAM WILL RESPOND TO ALL COMMENTS RECEIVED. COMMENTS MUST BE RECEIVED BY OCTOBER 7, 2023, TO BE CONSIDERED AS PART OF THE PUBLIC RECORD.

ALL CORRESPONDENCE REGARDING THIS PROJECT SHOULD BE SENT TO: IDOT DISTRICT 3

ATTENTION MR. KENNETH BANGA
700 EAST NORRIS DRIVE
OTTAWA, IL 61350

(FAX (815) 434-8553) OR (PHONE (815) 434-8469) (TTY (800) 526-0844).

(Published in The Times September 7, 2023) 2106792

Banga, Kenneth T.

From: Jackson Hurst <ghostlightmater@yahoo.com>
Sent: Wednesday, August 30, 2023 2:06 PM
To: Banga, Kenneth T.
Subject: [External] U.S. 52 Fox River Bridge Replacement Study

Follow Up Flag: Follow up
Flag Status: Flagged

Hi I would like to sign up for study updates and be added to the mailing list for the U.S. 52 Fox River Bridge Replacement Study. My mailing address is 4216 Cornell Crossing NW, Kennesaw GA 30144.

Sent from ghostlightmater@yahoo.com



Illinois Department of Transportation

Office of Highways Project Implementation / Region 2 / District 3
700 East Norris Drive / Ottawa, Illinois 61350-1628

July 11, 2024

CERTIFIED MAIL NO.:
7015 0640 0004 0139 6309

Jackson Hurst
4216 Cornell Crossing NW
Kennesaw, GA 30144

No additional response to date.

FAP 607 (US 52)
Section (125)BR-1
LaSalle County
Job No. P-93-015-17
Contract No. 66F85

Dear Mr. Hurst:

The Illinois Department of Transportation is in the preliminary engineering phase of a study concerning the spot improvement on US Route 52 located at approximately 2.1 miles west of IL Route 71 in LaSalle County (see attached location map). The proposed project consists of a complete structure replacement of a bridge (SN 050-0058) carrying US Route 52 over the Fox River, as well as improvements to the adjoining roadway approaches.

The project is tentatively scheduled in the department's *Fiscal Year 2025-2030, Proposed Highway Improvement Program*, subject to project readiness and funding availability.

SN 050-0058 is eligible for listing in the National Register of Historic Places (NRHP) as a Concrete Arch Deck, Open Spandrel bridge and is therefore accorded protection under Section 106 of the National Historic Preservation Act as well as Section 4(f) of the National Environmental Policy Act. On May 16, 2024, the State Historic Preservation Officer (SHPO) has concurred with the determination that the proposed project will adversely affect the historic bridge. Therefore, the department is currently developing a draft Memorandum of Agreement (MOA) that would stipulate the agreed upon mitigation measures for the adverse effects.

Based on an email the department received from you on August 30, 2023, you have asked to be added to the mailing list for this project. Attached to this letter are two copies of a response sheet. You may fill out this sheet to comment or request further discussions. Please indicate on the response sheet the appropriate reply and return the sheet to us in the enclosed, self-addressed, stamped envelope. Retain the second sheet for your personal records. If no response is received by **August 4, 2024**, it will be construed as a "no comment" response.

Jackson Hurst
Page 2
July 11, 2024

Upon completion and approval of our study, we will proceed with the plan preparation and land acquisition phase. To learn more about the project status and additional information, please visit <https://idot.illinois.gov/transportation-system/transportation-management/featured-projects/u-s--52-in-lasalle-county---fox-river-bridge.html>.

If you have any questions or wish to discuss the improvement in more detail, please contact Kenneth Banga, Location and Environmental Studies Engineer, at (815) 434-8469.

Sincerely,

Trisha Thompson, P.E.
Region Two Engineer

Dave Alexander KTB

By: David S. Alexander, P.E.
Studies & Plans Engineer

APPENDIX E

MEMORANDUM OF AGREEMENT (MOA)

**MEMORANDUM OF AGREEMENT
AMONG THE
FEDERAL HIGHWAY ADMINISTRATION,
ILLINOIS STATE HISTORIC PRESERVATION OFFICER,
AND
ILLINOIS DEPARTMENT OF TRANSPORTATION
REGARDING
REPLACEMENT OF THE US 52 BRIDGE
OVER THE FOX RIVER IN
LASALLE COUNTY, ILLINOIS**

WHEREAS, the Illinois Department of Transportation (IDOT) plans to replace the US 52 Bridge (Structure #050-0058) over Fox River in LaSalle County, Illinois (IDOT Sequence #20206, SHPO Log #003041924); and

WHEREAS, the Federal Highway Administration (FHWA) may fund the Project thereby making the Project an undertaking subject to review under Section 106 of the National Historic Preservation Act (NHPA), 54 U.S.C. Section 306108, and its implementing regulations, 36 C.F.R. Part 800; and

WHEREAS, the FHWA has defined the undertaking's architectural area of potential effects (APE) as a 0.25-mile buffer surrounding the historic bridge as shown in Exhibit A; and

WHEREAS, the FHWA has consulted with the Illinois State Historic Preservation Office (SHPO), a Division of the Illinois Department of Natural Resources (IDNR), pursuant to the Act; and

WHEREAS, the SHPO currently resides within IDNR (Office), and the Director of IDNR is the duly designated State Historic Preservation Officer (Officer); and

WHEREAS, the FHWA and IDOT, in consultation with the Officer, have determined that the US 52 Bridge over the Fox River (Structure #050-0058) is eligible for the National Register of Historic Places (NRHP), and its replacement will cause an adverse effect to this historic property pursuant to 36 C.F.R. Part 800 (Exhibit B); and

WHEREAS, the FHWA has determined that the undertaking will not have an effect on historic properties other than the US 52 Bridge, and the Officer concurred with this finding on May 16, 2024; and

WHEREAS, the FHWA and IDOT invited the following Tribes to enter consultation on September 26, 2016: Ho Chunk Nation, Miami Tribe of Oklahoma, Kickapoo Tribe in Kansas, Sac and Fox Nation, Pokagon Band of Potawatomi Indians, and Peoria Tribe of Indians of Oklahoma. The Miami Tribe of Oklahoma accepted consulting party status but expressed no specific concerns; and

WHEREAS, the FHWA and IDOT provided the public opportunities to comment on the undertaking and to become consulting parties. Engagement efforts included: (1) August and September 2023 newspaper notices in the LaSalle News Tribune and the Ottawa Times respectively, and (2) May 2023 letters sent to Landmarks Illinois, Historic Bridge Foundation, HistoricBridges.org, LaSalle County Historical Society, Sheridan Historical Society, Central Illinois Landmarks Foundation, and Northern Illinois Historic League. Comments were received from the Northern Illinois Historic League and a private citizen named Jackson Hurst. Follow-up letters were sent out, but no comments were received; and

WHEREAS, the IDOT, on behalf of FHWA, in accordance with 36 C.F.R. Part 800.6(a)(1), notified the ACHP of the adverse effect in an email dated August 28, 2024, and the ACHP declined to participate in consultation on September 11, 2024; and

WHEREAS, execution and implementation of this MOA evidences that FHWA has satisfied its Section 106 responsibilities for the Project; and

NOW, THEREFORE, FHWA, IDOT, and Officer agree that the Project shall be implemented in accordance with the following stipulations to ensure that potential effects on historic properties are taken into account.

STIPULATIONS

The FHWA, IDOT, and Officer agree that the following steps will be undertaken for the Project:

I. ARCHITECTURAL AND HISTORICAL RESOURCE INVESTIGATIONS

- A. Historic Illinois Engineering Record (HIER) Level III Recordation.** Prior to beginning of construction activities, the IDOT shall complete Historic Illinois Engineering Record (HIER) Level III recordation of the existing Bridge. The IDOT will submit the draft HIER (95%) to the Officer, and the IDOT must receive written concurrence from the Officer that the draft HIER documentation is acceptable prior to the IDOT initiating demolition of the Bridge. The IDOT will ensure the final HIER (100%) documentation is submitted to the Officer for filing.
- B. Video Documentation and Dissemination.** The IDOT in consultation with the Officer shall develop a video that highlights the bridge itself, as well as the historical importance of US 52 and the Fox River to the development of nearby communities. The video will be 5-10 minutes in length and will be provided in a format easily accessible to the public and historic bridge enthusiasts. The IDOT shall submit to the Officer for review the draft video within twelve (12) months of the removal of the US 52 bridge. The IDOT will ensure the final video is submitted to the Officer for approval and is then provided to consulting parties and other interested groups.

II. PROFESSIONAL STANDARDS

For the purpose of implementing this MOA, the IDOT shall continue to employ departmental staff with qualifications that meet the requirements of 36 C.F.R. Part 61. The IDOT shall ensure that the professional staff responsible for the implementation of the mitigation measures meet the requirements of 36 C.F.R. Part 61.

III. DURATION

This MOA will expire if its stipulations are not carried out within five (5) years from the date of its execution, unless otherwise amended or terminated pursuant to Stipulations VI or VII. In such an event, the IDOT shall so notify the parties to this MOA and, if it chooses to continue with the Project, then the FHWA shall reinitiate review of the Project in accordance with 36 C.F.R. Part 800.

IV. POST-REVIEW DISCOVERIES

If potential historic properties are discovered or unanticipated effects on historic properties found, the FHWA shall make reasonable efforts to avoid, minimize, or mitigate adverse effects to such properties and follow the requirements of 36 C.F.R. Part 800.13(b).

V. DISPUTE RESOLUTION

Should any signatory to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FHWA shall consult with such party to resolve the objection. If FHWA determines that such objection cannot be resolved, FHWA will:

- A. Forward all documentation relevant to the dispute, including any timely advice or comments regarding the dispute from the ACHP and signatories and the FHWA's proposed resolution, to the ACHP in accordance with 36 C.F.R. Part 800.2(b)(2). The ACHP shall provide FHWA with its advice on the resolution of the objections within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FHWA shall prepare a written response that takes into account any timely advice or comments regarding the dispute from the ACHP and signatories and provide them with a copy of this written response. FHWA will then proceed according to its final decision.
- B. If the ACHP does not provide its advice regarding the dispute within the thirty (30) day time period FHWA may make a final decision on the dispute and proceed accordingly. Prior to reaching such a final decision, FHWA shall prepare a written response that takes into account any timely comments regarding the dispute from the signatories to the MOA and provide them and the ACHP with a copy of such written response.

- C. FHWA's responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remain unchanged.

VI. AMENDMENTS

This MOA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all of the signatories is filed with the ACHP.

VII. TERMINATION

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment. If within thirty (30) days an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories. Once the MOA is terminated and prior to work continuing on the undertaking, FHWA must request, take into account, and respond to the comments of the ACHP under 36 C.F.R. Part 800.7. FHWA shall notify the signatories as to the course of action it will pursue.

Execution of this MOA by the FHWA, IDOT, and Officer and implementation of its terms evidence that FHWA has taken into account the effects of this undertaking on historic properties and afforded the ACHP an opportunity to comment.

(Signature Pages Follow)

**MEMORANDUM OF AGREEMENT
AMONG THE
FEDERAL HIGHWAY ADMINISTRATION,
ILLINOIS STATE HISTORIC PRESERVATION OFFICER,
AND
ILLINOIS DEPARTMENT OF TRANSPORTATION
REGARDING
REPLACEMENT OF THE US 52 BRIDGE
OVER THE FOX RIVER IN
LASALLE COUNTY, ILLINOIS**

Signatory

FEDERAL HIGHWAY ADMINISTRATION

By: *Darien J. Siddall* Date: 5/28/2025

Print Name:

Title: Environmental Protection Specialist

**MEMORANDUM OF AGREEMENT
AMONG THE
FEDERAL HIGHWAY ADMINISTRATION,
ILLINOIS STATE HISTORIC PRESERVATION OFFICER,
AND
ILLINOIS DEPARTMENT OF TRANSPORTATION
REGARDING
REPLACEMENT OF THE US 52 BRIDGE
OVER THE FOX RIVER IN
LASALLE COUNTY, ILLINOIS**

Signatory

ILLINOIS STATE HISTORIC PRESERVATION OFFICER

By: Carey L. Mayer Date: 5/22/2025

Print Name: Carey L. Mayer, AIA

**Title: Deputy State Historic Preservation Officer
Illinois Department of Natural Resources**

**MEMORANDUM OF AGREEMENT
AMONG THE
FEDERAL HIGHWAY ADMINISTRATION,
ILLINOIS STATE HISTORIC PRESERVATION OFFICER,
AND
ILLINOIS DEPARTMENT OF TRANSPORTATION
REGARDING
REPLACEMENT OF THE US 52 BRIDGE
OVER THE FOX RIVER IN
LASALLE COUNTY, ILLINOIS**

Invited Signatory

ILLINOIS DEPARTMENT OF TRANSPORTATION

By: Trisha Thompson Date: May 23, 2025

Print Name: Trisha Thompson

Title: Region 2 Engineer



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Bridge

Resources

APE

APE Map

Bridge Replacement
FAP 607 / US 52 over Fox River
Serena, Sheridan, Norway, LaSalle County, Illinois
IDOT Sequence #20206

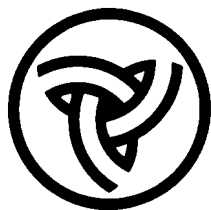
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Feet

LOCHMUELLER
GROUP

319 E Madison Street, Suite #2E
Springfield, Illinois
PHONE: 800.423.7411

Created: 3/15/2024, HBLAD



Illinois Department of Transportation

Memorandum

To: Trisha Thompson Attn: Roger Rynke
From: Jack Elston By: Joseph Galloy
Subject: Adverse Effect – SHPO Coordination Required
Date: June 20, 2024

LaSalle County
Serena, Sheridan, Norway
FAP 607 / US 52
Bridge Replacement
Structure # 050-0058
Contract # 66F85
IDOT Sequence # 20206
SHPO Log # 003041924

Further coordination with the Illinois State Historic Preservation Officer (SHPO) is required for the above referenced undertaking. Attached is a letter from the SHPO indicating that the undertaking as currently planned will constitute an Adverse Effect pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended, as it will adversely affect Structure # 050-0058, which is a historic bridge that is eligible for listing on the National Register of Historic Places (NRHP).

As a next step, we have received the Section 4(f) report and comments will be returned to your office shortly. IDOT Cultural Resources Unit will also develop a draft memorandum of agreement (MOA) that will contain mitigation that must be completed before any construction can be initiated.

A handwritten signature in cursive script, reading 'Joseph M. Galloy'.

Joseph M. Galloy, PhD, RPA
Cultural Resources Unit Manager
Bureau of Design & Environment

JG:el
Attachment



Illinois
Department of
**Natural
Resources**

JB Pritzker, Governor • Natalie Phelps Finnie, Director
One Natural Resources Way • Springfield, Illinois 62702-1271

www.dnr.illinois.gov

LaSalle County

Serena, Sheridan, Norway

Bridge Replacement, FAP 607/U.S. 52 over the Fox River (SN 050-0058)

IDOT-P-93-015-17, IDOT Seq #-20206

SHPO Log #003041924

May 16, 2024

Joseph Galloy

Illinois Department of Transportation

Bureau of Design and Environment

2300 S. Dirksen Parkway

Springfield, IL 62764

Thank you for your submission of the proposed bridge replacement project in LaSalle County, Illinois, which we received on 4/19/24 (SHPO Log# 003041924). Our comments are required by Section 106 of the [National Historic Preservation Act of 1966](#), as amended, 54 U.S.C. § 306108, and its [implementing regulations](#) (36 CFR Part 800) (Act).

We concur with the established Area of Potential Effects (APE) and the identification of historic properties within the APE:

- FAP 607/US 52 over Fox River, Sheridan (SN 050-0058). Previously determined eligible for listing on the National Register of Historic Places (NRHP), as an alternate on IDOT's Historic Bridge List.

Additionally, our staff have determined that no historic archaeological properties are known to exist within the APE. However, if any archaeological materials are encountered during construction, this office must be notified. This letter is not a clearance for purposes of the [Illinois Human Remains Protection Act \(20 ILCS 3440\)](#).

Lastly, we concur with the determination that the proposed project will adversely affect the historic bridge (SN 050-0058). Please work with CJ Wallace (Carol.Wallace@Illinois.gov) in our office to avoid, minimize, or mitigate the adverse effects pursuant to the Act.

Sincerely,

Carey L. Mayer, AIA

Deputy State Historic Preservation Officer