

United States Department of the Interior
National Park Service

National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. **Place additional certification comments, entries, and narrative items on continuation sheets if needed (NPS Form 10-900a).**

1. Name of Property

historic name Wayne Depot

other names/site number Chicago and North Western Railroad Depot

Name of Multiple Property Listing N/A

(Enter "N/A" if property is not part of a multiple property listing)

2. Location

street & number 32W305 Army Trail Rd. ☐ not for publication

city or town Wayne ☐ vicinity

state Illinois county DuPage zip code 60184

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

I hereby certify that this ___ nomination ___ request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property ___ meets ___ does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance: ___ national ___ statewide ___ local

Applicable National Register Criteria: ___ A ___ B ___ C ___ D

Signature of certifying official/Title: Deputy State Historic Preservation Officer Date

Illinois Department of Natural Resources - SHPO
State or Federal agency/bureau or Tribal Government

In my opinion, the property ___ meets ___ does not meet the National Register criteria.

Signature of commenting official Date

Title State or Federal agency/bureau or Tribal Government

4. National Park Service Certification

I hereby certify that this property is:

___ entered in the National Register ___ determined eligible for the National Register

___ determined not eligible for the National Register ___ removed from the National Register

___ other (explain:)

Signature of the Keeper

Date of Action

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5. Classification

Ownership of Property
(Check as many boxes as apply.)

<input checked="" type="checkbox"/>	private
<input checked="" type="checkbox"/>	public - Local
<input type="checkbox"/>	public - State
<input type="checkbox"/>	public - Federal

Category of Property
(Check only **one** box.)

<input checked="" type="checkbox"/>	building(s)
<input type="checkbox"/>	district
<input type="checkbox"/>	site
<input type="checkbox"/>	structure
<input type="checkbox"/>	object

Number of Resources within Property
(Do not include previously listed resources in the count.)

Contributing	Noncontributing
1	buildings
	site
	structure
	object
1	Total

**Number of contributing resources previously
listed in the National Register**
N/A

6. Function or Use

Historic Functions
(Enter categories from instructions.)

TRANSPORTATION: Rail-related

Current Functions
(Enter categories from instructions.)

SOCIAL: Meeting Hall
RECREATION AND CULTURE

7. Description

Architectural Classification
(Enter categories from instructions.)

LATE VICTORIAN: Shingle Style

Materials
(Enter categories from instructions.)

foundation: concrete
walls: clapboard

roof: Cedar shingles
other:

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Narrative Description

(Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity).

Summary Paragraph

The Wayne Depot, built in 1884, is located in downtown Wayne, an equestrian village straddling DuPage and Kane counties in northeastern Illinois. The depot was designed by Chicago architects Cobb & Frost in the Shingle Style, with shingles prominently displayed in the gable ends and dormer window. In 1950, after passenger service stopped, the station was moved to private property to save it from demolition. In 2007 the Wayne Depot was returned to its original location alongside the Chicago and Northwestern (now Union Pacific) Railroad tracks. The nomination consists of one contributing property, the depot.

Narrative Description

Wayne Depot is located southwest corner of Army Trail Road and the Union Pacific railroad tracks. The Wayne Village Historic District (1978) is east of the depot, across the tracks. The district is largely residential, with a small commercial corridor along the east side of the railroad tracks. The depot faces east and has no landscaping or hardscaping. Behind the depot is the Dunham Forest Preserve Parking Lot & Trailhead.

The depot is one-and-a-half stories tall and sits on a concrete foundation. Its footprint is mostly rectangular, with a two-story, five-sided bay window in the center of the façade that continues through the roof. The high-pitched, side gabled roof is connected to a shed roof that covers the porch. The roof is clad with wood shakes and upper level is clad with shingles. The walls are lap siding on the lower half and vertical beadboard siding on the upper half, divided by a band of trim. The building has four 8-foot, six-panel doors with brass hinges and hardware; two on the front and two on the back elevation.

The focal point of the façade is the two-story bay window, which, on the upper level which has a 36-pane, true divided light, single sash window. The sides and corners of the bay are clad with shingles in a coursed pattern with the top row in a diamond-shaped pattern. The dormer roof is wood shake with flared edges. The bay is located at the juncture of the gable and shed roof, which covers the porch. The shed roof is supported by four porch posts that are square in section and have simple brackets. The windows on the first floor of the bay have eighteen over two lights. There are two entrance doors on the façade, one on either side of the bay. Windows at the north and south ends of the façade have twelve over two lights.

The north and south elevations are identical. Most of the shingles are in a coursed pattern except for a band of diamond-shaped shingles. The bottom row of shingles is also in the diamond-shaped pattern. Each with eyebrow windows beneath the roof's peak and Palladian windows directly below. The eyebrow windows are narrow and divided by a wide mullion. They have three lights each. The arch of the Palladian window is spoked, the center window is eighteen over two and the side windows are nine over one. The rear elevation has a double-sash window with twelve over one lights on the north and south ends and two doors in the center.

The interior of the building has an open floor plan, with only a 4' wide staircase hidden behind a knee wall for access to a full basement that houses mechanicals. It has a beadboard ceiling and walls, with chair

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rail that wraps around the room's perimeter. The windows and doors have plain capped trim, with beaded casings. The baseboards have square edges, and the ceiling moldings have simple convex profiles. The doors have six horizontals, or cross, panels and decorative push plates with handles and deadbolts. The subfloor is exposed.

The north wall has a three-sided bay window with a multi-paned dormer window above the center window. The octagonal-shaped recess in the ceiling delineates the shape of the dormer. On either side of the bay is a door and a window. The east and west walls each have large Palladian windows. The south wall has two doors in the center with windows on either side.

The full basement has an ADA accessible bathroom and storage space for the extensive Dunham collection that was accessioned by the Wayne Historical Preservation Society (WHPS) in 2010. The electrical, HVAC and plumbing mechanicals, are also located in the basement.

Integrity

The Wayne Depot has sufficient integrity for listing in the National Register of Historic Places. It has undergone an extensive restoration with care taken to repair or replace in kind those features that been damaged over time. While on private property, the depot had fallen into serious disrepair. It was used first as a garage, with the opening of the west elevation to include four garage doors. The last owners of the property used it for storage and as a barn that included a horse stall.

In 1993, as part of its historic preservation program, the Village of Wayne acquired the lot where the depot originally stood from the Chicago and Northwestern Railroad. In 2002, the Wayne Historic Sites Commission designated the Depot as a local historic landmark because of its special historic and aesthetic value to the community. Through WHPS fundraising efforts and strong community support, by 2007 sufficient money had been raised to complete Phase I of the Depot Move and the structure was returned to its exact location alongside the Chicago and North Western railroad tracks. Before the owners donated the property to the WHPS the rotted portico structure was restored, and the roof was replaced matching the cedar shingles that were original to the building.

The clapboard was deteriorated beyond repair and was replaced with reproduction materials made to match the original. Most of the shingles on the gable ends were intact and retained. Those that were damaged were replaced as needed, each individually crafted to match original and blend in with existing. All exposed nailing was done with square cut nails to match those used in the original construction.

The original Palladian windows on the north and south ends of the building were intact, but roughly 40% of the Depot's windowpanes were missing. Replacement pieces were cut from reclaimed glass from original storm windows from the Dunham Castle (built in 1882) which matched the irregularity of glass from the era.

The modern garage doors that had been installed on the west side, were removed and two, six paneled doors that were originally there were reinstalled, using the original doors on the east side as models. Brass hardware on doors and windows was replaced with reproduction brass hardware appropriate of the era. The surviving damaged hardware was used to locate accurate reproductions and then kept for exhibit.

Paint chips were taken from the original beadboard and professionally scanned to replicate the original color of the interior walls; paint chips were taken from the windows and doors to replicate that color, and the original exterior paint color of the building was discovered through paint chips as well.

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The interior walls and ceiling also experienced considerable deterioration. They were removed and replaced with Douglas Fir custom-dimensioned beadboard, replicating the pieces that had been saved for that purpose. No original flooring survived. There is still uncertainty as to what the original flooring was. Other depots of the era were visited to determine whether wide or thin floorboards were more prevalent. So far, none have retained the original flooring.

Roughly 40% of the original interior trim around windows and doors was retained; where new pieces needed to be added, dimensions were reproduced from surviving trim.

The Depot was originally heated by a stove located in the station agent's bay. The new forced air system required grates to cover ductwork openings on the ceiling. Ten 1880s-style reproduction heat grate covers (exact copies of those used in Carnegie libraries of the same era) were installed for feed and return of conditioned air.

The building's electrical, HVAC and plumbing mechanicals, are located in a full basement that replaced the vented narrow crawl space. The basement also houses an ADA accessible bathroom and storage space for the extensive Dunham collection that was accessioned by the Wayne Historical Preservation Society (WHPS) in 2010.

The Depot was built with two waiting rooms separated by a wall, conceivably one for men and the other for women. A middle ticket area had one access door on the north end, a stove, and was surrounded by short walls as seen in the floor plan in the Inland Architect. These walls were not reinstalled. The main floor now has an open floor plan. The heating stove, partition walls that enclosed the ticket office, and the center wall were removed, but the octagonal recess in the ceiling that delineates the space of the original ticket office was retained. A new 4' wide staircase was added on the north end hidden behind a knee wall for access to the basement.

The restoration has been done in stages based upon available funds that were raised for each project, described per year below:

2007: The site was prepared for the move. Work entailed excavating, pouring a concrete foundation and floor slab, and backfilling.

2008 and 2009: Old interior paneling and other wood elements, including 4 non-original garage doors from west wall, were removed from the building. The interior framing was replaced and reinforced and the octagonal ceiling opening was rebuilt as necessary. Old electrical equipment was removed and electric service and telephone cable was installed. The front door and windows were stripped and temporary windows were installed as necessary.

The shrubs and trees were cleared from the lot and the entire site was regraded. The well and septic were installed. On the depot, the porch brackets were replaced with ones that replicated the originals. Repaired and replaced diamond-shaped shingles on both gable ends.

2010: Thirteen windows were repaired and reglazed, with new reproduction wavy glass installed as needed. The molding that was missing from the original large door was replaced. Custom built wood jambs, thresholds; installed doors and weather stripping. Repaired and replaced siding and trim; paint matching original color.

2011: Two new replicas of the original six-panel doors were installed on the west wall.

2012: Bathroom framing upstairs.

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2014: On the site, fencing was installed around septic field and the temporary brick pavers were installed. Painting trim.

2015: Installed closed-cell foam insulation in the roof. The trim on the windows was repainted.

2016: The HVAC, the electric, and the plumbing --which was relocated to the basement--were roughed in. The doors were hung with reproduction hardware. The basement walls were insulated and had wood framing installed for drywall attachment.

2019: The water and gas pipes were roughed in as was the electrical wiring. A sump pump was installed. Drywall and taping were completed. Doors and windows and casings were installed. The exterior was painted.

2021: Closed-cell foam insulation was installed in the walls.

2023: The gas line was connected and a back-up generator was installed. Recreated original and installed trim details: ceiling and wall bead board, baseboards, window casings, door casings and wall to ceiling transition.

2024: Ductwork was added in the attic and the furnace and AC were installed in the basement. Plumbing trim was added in the furnace room basement. Floor tile was installed in the bathroom and at the base of the steps in basement.

2025: The ceiling and walls were painted. Paint chips from the original beadboard were used to determine the original color.

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8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- ☒ A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- ☐ B Property is associated with the lives of persons significant in our past.
- ☒ C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- ☐ D Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply.)

Property is:

- ☐ A Owned by a religious institution or used for religious purposes.
- ☐ B removed from its original location.
- ☐ C a birthplace or grave.
- ☐ D a cemetery.
- ☐ E a reconstructed building, object, or structure.
- ☐ F a commemorative property.
- ☐ G less than 50 years old or achieving significance within the past 50 years.

Areas of Significance

(Enter categories from instructions.)

Transportation

Architecture

Period of Significance

1884-1938

Significant Dates

Significant Person

(Complete only if Criterion B is marked above.)

Cultural Affiliation (if applicable)

Architect/Builder

Cobb & Frost, Architects

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Statement of Significance Summary Paragraph (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations).

The Wayne Depot is locally significant and eligible for listing in the National Register of Historic Places under Criterion A: Transportation for its association with the Village of Wayne's railway history. Initially built for renowned Village resident Mark Dunham to receive the Percheron horses he purchased in France; the depot also played an important role in the community's development. The period of significance for transportation is from 1884, the year it was constructed, until 1938, the year passenger service stopped.

Wayne Depot is eligible under Criterion C: Architecture, as an important example of the Shingle Style. Designed by architects Cobb & Frost, the depot is a recognizable example of their work with a "twin sibling" just 12 miles away in Wheaton. A drawing of the Depot is featured in the Inland Architect and News Record and the Railway Age of 1887-1888 alongside other Cobb and Frost depots. The period of significance for architecture is 1884, the year it was built. The depot does not have to meet Criteria Consideration B: Moved Properties for it was returned to its original location next to the CN&W railroad tracks in Downtown Wayne.

Narrative Statement of Significance (Provide at least **one** paragraph for each area of significance.)

HISTORY OF WAYNE

There are three clearly defined eras in the development of the Village of Wayne. The early years, 1834-1869, brought settlers from the east, mostly New England and many of them veterans of the Black Hawk War, which was fought in the Fox Valley, who were attracted by the abundance of land and its prime location along the Fox River. These early pioneers contributed to the agricultural development of the area.

The second era, 1870-1927, saw the area move into the age of industrialization. As the farmsteads increased in size, new equipment was developed to assist the farmers in working their land with increased efficiency. The Dunham family recognized the need for a strong draft-type horse to pull this equipment and capitalized upon it by introducing the Percheron horses for which Oaklawn Farm became legendary. The Dunhams were instrumental in encouraging the use of mechanization in American farming, the very thing that would eventually lead to the demise of their operation and the subsequent development of their pastures and farmland into real estate holdings.

Though the breeding operation came to an end in 1923, when the last Dunham Percheron was sold, the Dunham heirs were careful to preserve the quiet equestrian feel of the 700 acres they were selling by creating a "covenant" in 1937 which required four-acre minimum lots, a minimum cost of home construction, a setback requirement for outbuildings, and a prohibition on commercial enterprises. The covenant was challenged but upheld in court in 1989. In 2016, the covenant was used to stop a developer from having a commercial venue in the Village.

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Following the Dunham era, the introduction of foxhunting in the early 1930s ensured that Wayne would remain a destination for horse enthusiasts. Many came from the City of Chicago to pursue this lifestyle. This period saw a great deal of new, though controlled, construction, as well as the adaptation of many original homesteads for modern use.

Today, the character of the Village reflects a blend of old and new. Horses remain integral to the Wayne lifestyle. As new homes have been added to the landscape, great care has been taken to retain the open space for which Wayne is famous by creating horse easements on estate properties and a strong Historic Sites Commission exhorts residents in town to maintain their historic homes in keeping with their position on the National Register.

HISTORY OF THE RAILROAD

The original charter of the Galena and Chicago Union Railroad was granted by the Illinois Legislature on January 16, 1836. Work was suspended in 1838 because of the depression of the previous year but eight years later, former Chicago mayor, William Ogden prodded the dream once again into reality.

The proposed railroad was to be built along the Galena Chicago state road. Scattered along the line of the proposed railroad were small settlements waiting for the time when they would become markets for agricultural produce and receive products from the rest of the country.

The organizers of the railroad, headed by Ogden, sold stock to the farmers and villagers of the region to raise money to start their venture, promising a farm to market enterprise. Their idea was to build as the money came in: 10, 20, 30 miles at a time to avoid debts and borrowing. The railroad would belong to the farmers. Work did not progress as rapidly as other railroads which were springing up all over the country, but the Galena never incurred debt, did not borrow money and did not receive government land grants. Other lines were receiving loans and grants which the Taylor administration hoped would foster immigration to the "new lands" of Iowa and Wisconsin that had just become states in 1846 and 1848 respectively.

In 1848, the Galena, as it had come to be called, would be completed as far as the Desplaines River and by 1849 would continue to Elgin passing through Wayne Station. At that time, construction would begin on a depot in Chicago but there was no mention of any other depots along the line.

Transportation

In the early 1800s, transportation routes developed through DuPage County (organized in 1839) because it lay between Chicago and two important Illinois trade centers: Ottawa to the southwest and Galena to the Northwest. The introduction of rail lines along these routes secured the connection between the farming settlements in DuPage and the Chicago transport network. The population of Wayne Township, in the far west corner of the County, grew because of this network.

Wayne Center was established in 1834, three miles east of the current location of the Village of Wayne. When William Ogden, Chicago's first mayor and president of the Galena and Chicago Railroad (G&C RR) was acquiring property for the railroad right-of-way, Solomon Dunham, one of the area's non-native settlers, deeded a portion his property in the western part of Wayne Township to the railroad. The land grant is dated 1849 and the subsequent warranty deed was issued in 1860. The railroad was routed to bypass Wayne

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Center, running three miles to the west, and led to the Center's decline. By 1853, Solomon had built depot on the east side of the tracks on his donated property in the newly formed community named Wayne Station. A post office was commissioned and operated out of the depot, and Solomon became first station master and postmaster. In a County School Commissioner's report of that same year, the depot was even referred to as Dunham's Depot.

In 1884 Solomon's son, Mark, built a new and much more spacious Depot on the west wide of the former G&C RR (which was reorganized and renamed Chicago and Northwestern), and Solomon's depot was relocated to the north and west of the Wayne Road to be used as a freight house.

During the first part of the century, agriculture was the most common industry in the area, and Wayne Station was the only rail shipping point in Wayne Township for local farmers to ship their produce to market. An article in the Elgin Advocate of 1876 states that 90 cans of milk a day were shipped out of the station. Farmers from across the township shipped their wheat, oats and corn to Chicago on the railroad cars. Though Wayne was an important transportation center, one historian suggests that Wayne did not develop into a larger city because of its proximity to the more commercial and populated City of Elgin.

The railroad played an important role in the development of Wayne Station, now known as the Village of Wayne, as an equestrian community. Across the developing nation, the farming communities required strong horses for plowing and other farm work. The most powerful and thus popular breeds were the French Percheron, the Belgian Draft, and the Clydesdale. When Mark Dunham acquired his first Percheron "Success" in 1875, and began the stock farm that eventually became, as his letterhead boasted "The Largest Importing and Breeding Establishment in the World", the railroad station was the end of the journey for not only the first almost two dozen Percherons brought to Wayne from La Perche district of Normandy, but for almost 1,400 others over the next ten years. Dunham's business became so prosperous that in time he had his own boxcars, with his name lettered on the side, to transport the animals to Wayne. Until 1923, when the Dunham horse-breeding enterprise ceased operations largely due to the introduction of the tractor, Percheron horses ended their long transatlantic journey at the Depot in Wayne.

According to C&NW train schedules, passenger trains continued to go through Wayne until 1950, but as of 1936 the depot was boarded up and the trains did not stop in the Village except to drop mail.

Architecture

The Wayne Depot is locally significant and eligible for listing in the National Register of Historic Places under Criterion C: Architecture as an important local example of the Shingle Style. The style, which dates from the late 1880s and early 1900s, was transitional and featured characteristics of both the Queen Anne and Colonial Revival styles. The Shingle Style was more commonly used for residential architecture, influenced by the summer vacation houses of affluent East Coasters, particularly the older New England Colonial homes. Characteristics of the Queen Anne Style found in Shingle Style architecture include irregular floor plans, shape and form, large porches, turrets, and multitude of intricate windows. Colonial Revival influences include the use of classical features, such as Palladian windows and columns. Noteworthy architects and architectural firms of the time that designed homes in the Shingle Style included H.H. Richardson, Frank Lloyd Wright, McKim, Mead & White and Peabody & Stearns.¹

The Shingle Style was considered to be distinctively American. Although examples can be found in houses throughout the country, it largely spread through architectural magazines and never was as prevalent as the Queen Anne style. Examples are typically architect-designed high-fashioned houses, not vernacular buildings.²

¹ Washington State Department of Archaeology and Historic Preservation, "Shingle Style, 1885 – 1910," Architectural Style Guide, accessed July 29, 2025. <https://dahp.wa.gov/historic-preservation/historic-buildings/architectural-style-guide/shingle-style>.

² McAlester, Virginia, et al. A Field Guide to American Houses: The Definitive Guide to Identifying and Understanding America's Domestic Architecture. Revised and expanded edition/second edition. Alfred A. Knopf, 2013, p. 374.

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It is easily identifiable by shingle cladding on walls and roofs, but other characteristics include irregular facades and rooflines, steep roof pitches, large porches, multi-light windows, and round or polygonal towers.³ The Wayne Depot is identified as an example of the Shingle Style by its continuous use of wood shingles on the upper level, eyebrow windows, Palladian windows, polygonal bay, and large porch beneath the main roof. Though the lower elevation is wood clapboard, the varied decorative shingles on either gable end are the defining feature.

While there are no other examples of Shingle Style in Wayne that compare to the depot, there are residences in the village that have characteristics of the style. Of those, the houses located at 5N323 Powis Road and 31W745 Army Trail Road are the best representations. Both have gambrel roofs and irregular rooflines, limited decorations, and an abundant use of shingles on the upper stories and porches. These houses were constructed in 1916 from a barn that was cut in half. The depot is similar to the Wheaton Depot, which is now part of Cosley Park.

Architects Cobbs and Frost

In 1882, Henry Ives Cobb partnered with Charles Sumner Frost to create Cobb and Frost- an architectural firm that led to the creation of some of Chicago's most signature looks. Cobb, originally born in New England, studied architecture at the Massachusetts Institute of Technology as well as Harvard University, eventually graduating as an engineer. His career began there, at the Boston architecture firm Peabody and Stearns. What brought him to Chicago was a need for a new clubhouse for the established Union Club in 1881. After overseeing the project, Cobb began a career in the Windy City. With an accomplishment like the Union Club clubhouse under his belt and an undoubtedly bustling career on the horizon, Cobb asked Frost to partner with him. Frost had also coincidentally worked for Peabody and Stearns.

Cobb's role was creating the foundation for the designs Cobb and Frost brought to life. Frost contributed to design but functioned prominently as a supervisor of the actual construction, drawing on his own engineering experience.

The partnership between the two architects ended just before 1889, as Cobb wanted to dedicate himself entirely to the commission for the Newberry Library. While Cobb received incredible and significant commissions after his partnership with Frost had come to an end, the firm Cobb and Frost undoubtedly laid the groundwork for his career- and the legacy they both left behind through design.

Conclusion

The Wayne Depot is a good candidate for listing in the National Register of Historic Places. The property was played an important part in the community's transportation history. It is also an important example of Shingle Style architecture. Its lengthy and thorough restoration was done with sensitivity, and the depot has maintained sufficient integrity for listing.

³ Pennsylvania Historical & Museum Commission, "Shingle Style 1880 - 1900," Pennsylvania Architectural Field Guide, accessed July 29, 2025. <https://www.phmc.state.pa.us/portal/communities/architecture/styles/shingle.html>

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9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form.)

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Vosefski, Greg Transportation and Community Development in DuPage County, Illinois 1830-1880 PhD thesis, Northern Illinois University 1987 Greg Vosefski

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Previous documentation on file (NPS):

☐ preliminary determination of individual listing (36 CFR 67 has been requested)
☐ previously listed in the National Register
☐ previously determined eligible by the National Register
☐ designated a National Historic Landmark
☐ recorded by Historic American Buildings Survey # _____
☐ recorded by Historic American Engineering Record # _____
☐ recorded by Historic American Landscape Survey # _____

Primary location of additional data:

☒ State Historic Preservation Office
☐ Other State agency
☐ Federal agency
☐ Local government
☐ University
☐ Other

Name of repository: _____

Historic Resources Survey Number (if assigned):

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10. Geographical Data

Acreage of Property Less than one

(Do not include previously listed resource acreage; enter "Less than one" if the acreage is .99 or less)

Latitude/Longitude Coordinates

Datum if other than WGS84: _____
(enter coordinates to 6 decimal places)

1	<u>41°.950226</u> Latitude	<u>-88°.249648</u> Longitude	3	_____ Latitude	_____ Longitude
2	_____ Latitude	_____ Longitude	4	_____ Latitude	_____ Longitude

Verbal Boundary Description (Describe the boundaries of the property.)

Lot 1 in the Village of Wayne Assessment plat number 1, being in the east half of section 18, Township 40 North Range 9 East of the Third Principal Meridian according to the Plat thereof recorded Nov 30, 1995, as document number R95-168277, in DuPage County, Illinois.

Northern edge along Army Trail Rd, western side abuts DuPage Forest Preserve property, south roughly 320' to the eastern edge along the Union Pacific Railroad line.

Parcel #0118400002

Boundary Justification (Explain why the boundaries were selected.)

It is part of the original property that Solomon Dunham donated for the right-of-way of the railroad track through Wayne.

11. Form Prepared By

name/title	<u>Karen Armbrust President</u>	date	_____
organization	<u>Wayne Historical Preservation Society</u>	telephone	<u>(630) 319 1877</u>
street & number	<u>2320 N Ill Rt 23. POB 2</u>	email	<u>armbrust@ameritech.net</u>
city or town	<u>Marengo</u>	state	<u>IL</u> zip code <u>60152</u>

Additional Documentation

Submit the following items with the completed form:

- **GIS Location Map (Google Earth or BING)**
- **Local Location Map**
- **Site Plan**
- **Floor Plans (As Applicable)**
- **Photo Location Map** (Include for historic districts and properties having large acreage or numerous resources. Key all photographs to this map and insert immediately after the photo log and before the list of figures).

Wayne Depot
Name of Property

DuPage County, Illinois
County and State

Local location map



Wayne Depot
Name of Property

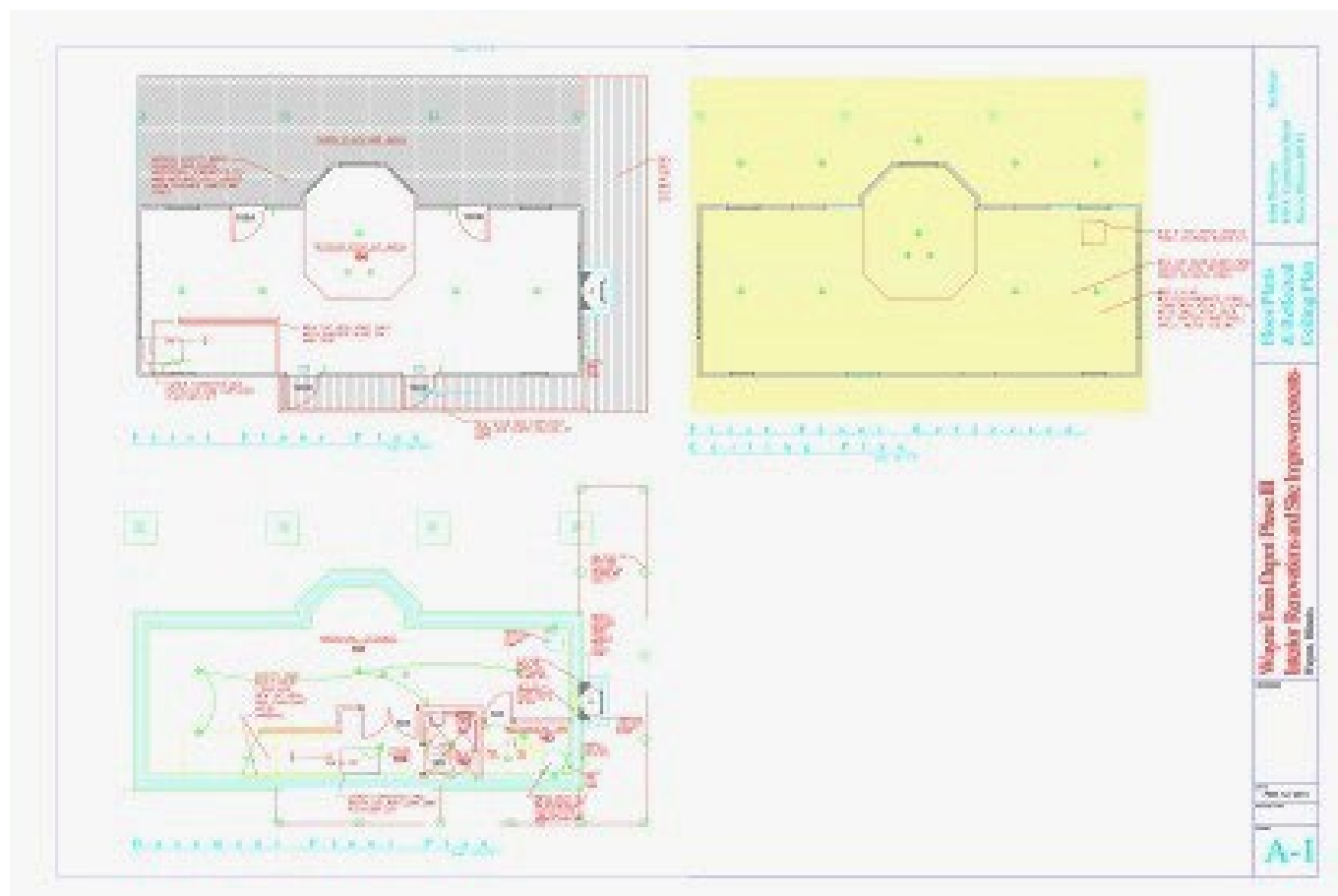
DuPage County, Illinois
County and State

GIS map



DuPage County, Illinois
County and State

Floor plan



Wayne Depot
Name of Property

DuPage County, Illinois
County and State

Photographs:

Submit clear and descriptive photographs. The size of each image must be 3000x2000 pixels, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered, and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

Photo Log

Name of Property:	<u>Wayne Depot</u>		
City or Vicinity:	<u>Wayne</u>		
County:	<u>DuPage</u>	State:	<u>Illinois</u>
Photographer:	<u>Dave Armbrust/Tully Williams</u>		
Date Photographed:	<u>Various</u>		

Description of Photograph(s) and number, include description of view indicating direction of camera:

Photo 1 of 9:	IL_DuPage County_Wayne Depot 0001	Exterior north elevation, camera facing South
Photo 2 of 9:	IL_DuPage County_Wayne Depot 0002	Exterior east elevation, camera facing West
Photo 3 of 9:	IL_DuPage County_Wayne Depot 0003	Exterior south elevation, camera facing North
Photo 4 of 9:	IL_DuPage County_Wayne Depot 0004	Exterior west elevation, camera facing East
Photo 5 of 9:	IL_DuPage County_Wayne Depot 0005	Close up North elevation, camera facing South
Photo 6 of 9:	IL_DuPage County_Wayne Depot_0006	Close up upper window on west elevation, camera facing east.
Photo 7 of 9:	IL_DuPage County_Wayne Depot_0007	Original door hardware
Photo 8 of 9:	IL_DuPage County_Wayne Depot_0008	Door with hardware
Photo 9 of 9:	IL_DuPage County_Wayne Depot_0009	Close up of two doors from West elevation, camera facing East

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

Estimated Burden Statement: Public reporting burden for this form is estimated to average 100 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Office of Planning and Performance Management, U.S. Dept. of the Interior, 1849 C. Street, NW, Washington, DC.

Wayne Depot

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List of Figures

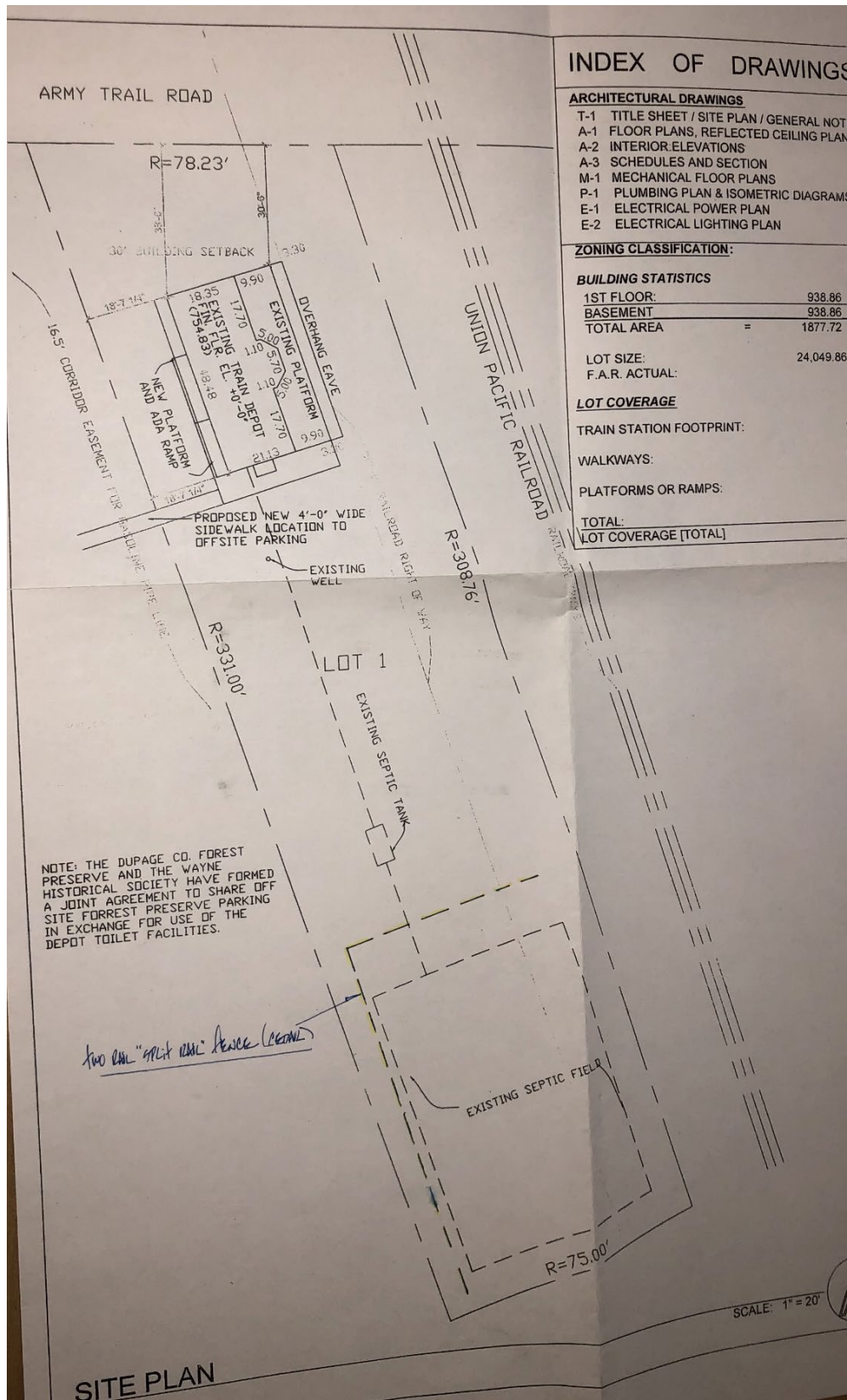
(Resize, compact, and paste images of maps and historic documents in this section. Place captions, with figure numbers above each image. Orient maps so that north is at the top of the page, all documents should be inserted with the top toward the top of the page.

Figure 1. Adam Glos survey of the Village of Wayne, 1893, showing location of Depot in SW corner of intersection of Main Street and the CN&W tracks.

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Figure 2. Wayne Depot plat map May 20, 2013



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Figure 3. Compass land survey 1993

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Figure 4. Page of The Inland Architect and News Record, No.6, Vol X 1887-1888

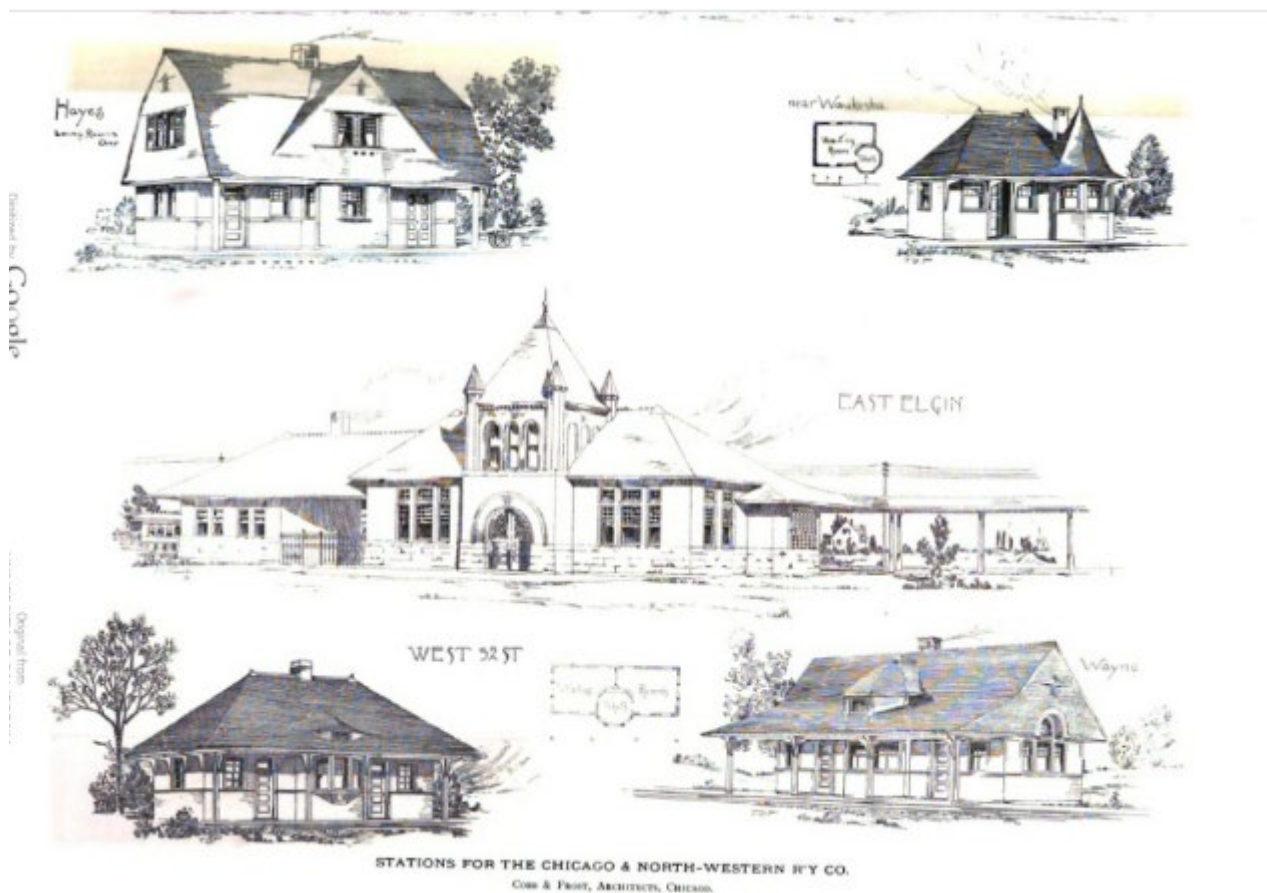


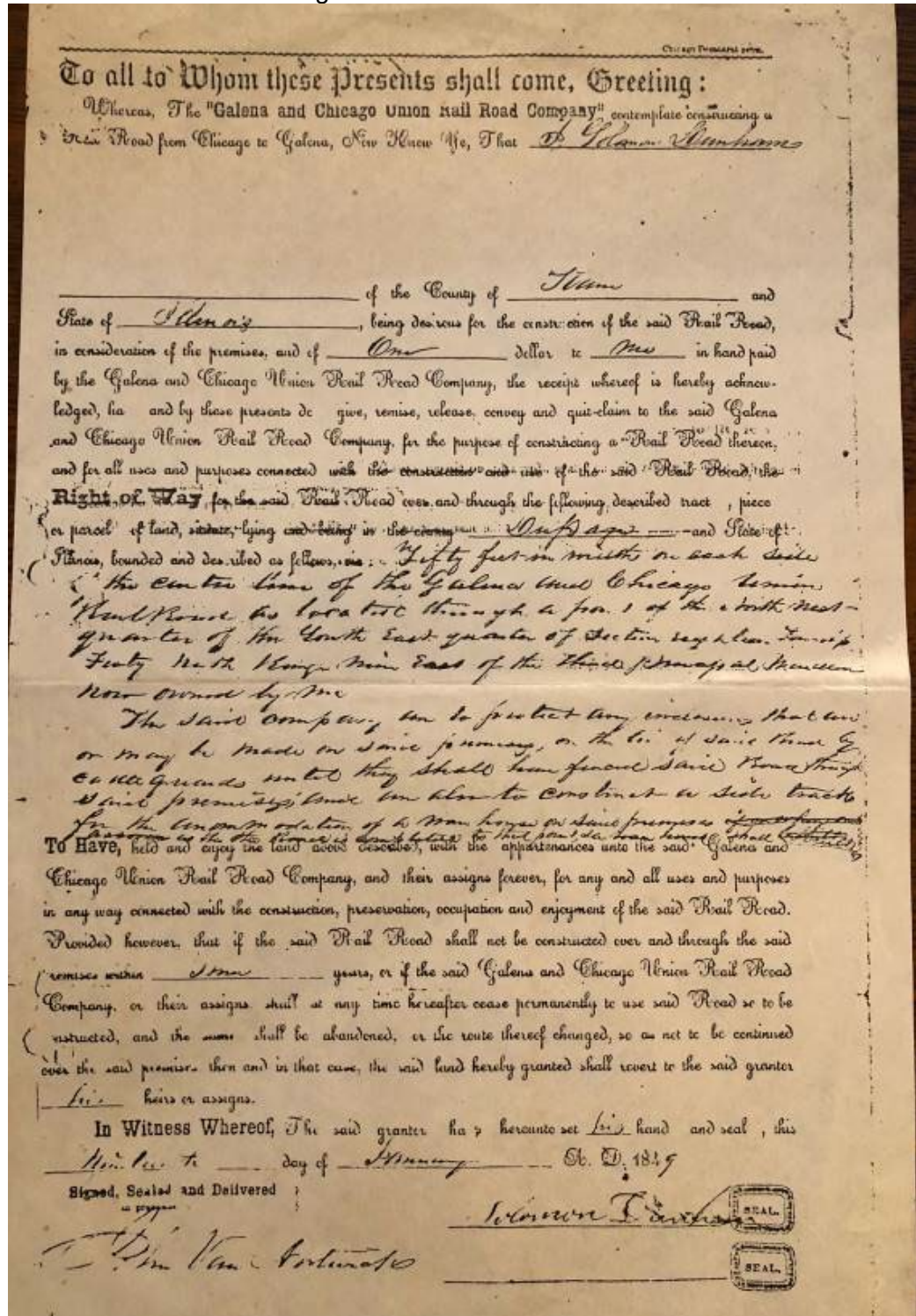
Fig 5. Closeup for floor plan



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Figure 6. Solomon Dunham land grant to the Union and Galena Railroad 1849



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Figure 7.

North elevation pre restoration, camera facing South



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Figure 8. Depot with Pioneer date unknown



Figure 9. Facing North, Depot with outhouse, date unknown



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Figure 10. South elevation Depot boarded up, taken June 7, 1936



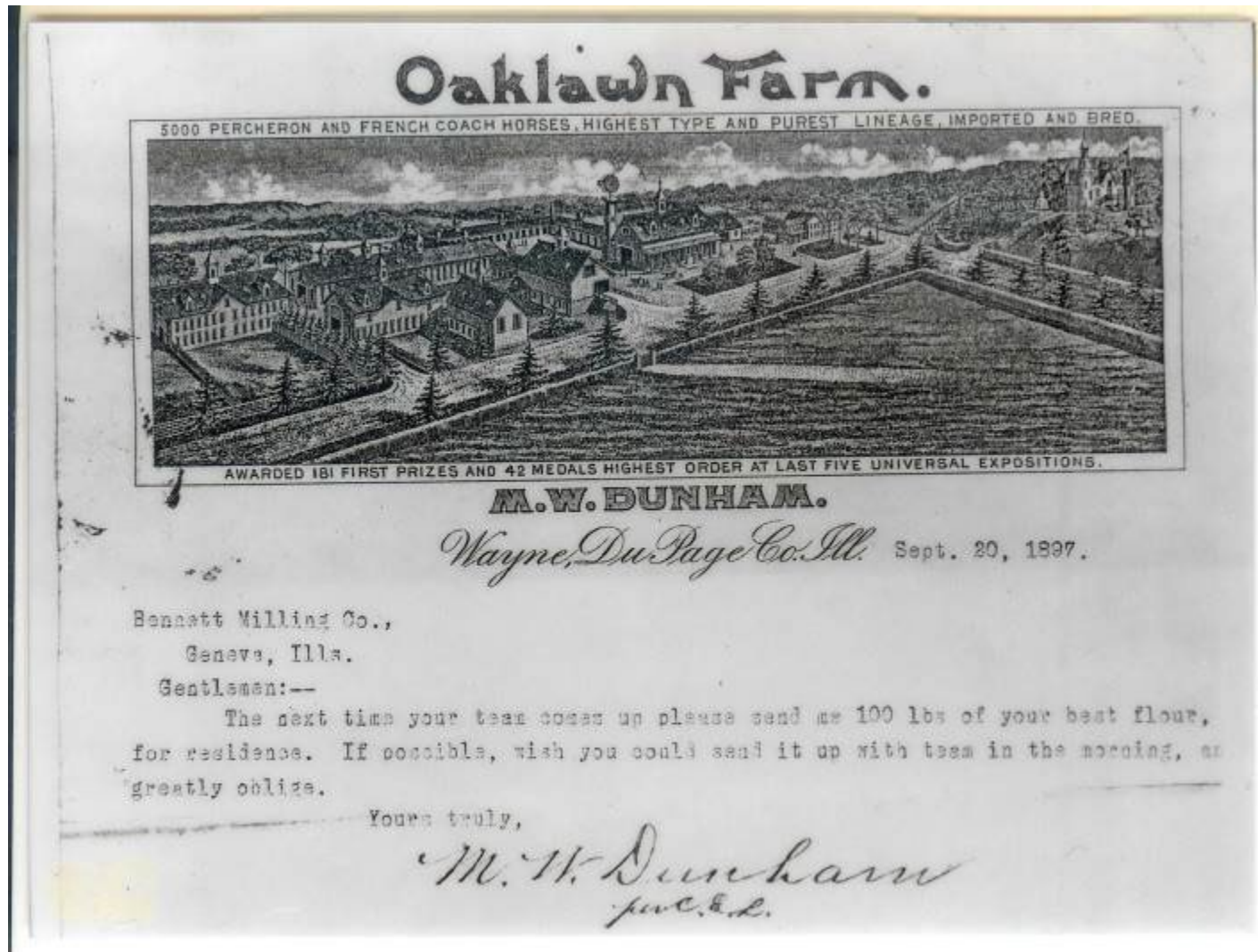
Figure 11. Oaklawn boxcar sketch in Dunham stud book



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Figure 12. Dunham letterhead



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Figure 13.

Prairie Farmer magazine 1886

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Figure 14. St. Charles Chronicle May 26, 1938



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Figure 15. Comparisons of similar styles in Wayne

31W745 Army Trail Rd



5N323 Powis Rd



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Photo 1 of 9: IL_DuPage County_Wayne Depot_0001 Exterior north elevation, camera facing South



Wayne Depot
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Photo 2 of 9: IL_DuPage County_Wayne Depot_0002 Exterior east elevation, camera facing West



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Photo 3 of 9: IL_DuPage County_Wayne Depot_0003 Exterior South elevation, camera facing North



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Photo 4 of 9: IL_DuPage County_Wayne Depot_0004 Exterior west elevation, camera facing East



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Photo 5 of 9: IL_DuPage County_Wayne Depot_0005 Close up North elevation camera facing South



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Photo 6 of 9: IL_DuPage County_Wayne Depot_0006

Close up upper window on west elevation, camera facing east.



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Photo 7 of 9: IL_DuPage County_Wayne Depot_0007

Original door hardware



Photo 8 of 9: IL_DuPage County_Wayne Depot_0008

Door with hardware



Wayne Depot
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Photo 9 of 9: IL_DuPage County_Wayne Depot_0009 Close up of two doors from West elevation, camera facing East



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Owner and Local Elected Official Notification Information

The State Historic Preservation Office is federally required to notify the property owner(s), as well as the chief elected officials of the county and municipal political jurisdiction in which the property is located. Please make sure the information is current.

Wayne Depot
32W305 Army Trail Rd.
Wayne, IL 60184

Name & Address of Property Owner. If there are multiple owners, you may need to send a separate spreadsheet:

PO Box 532
Building: Wayne Historical Preservation Society
PO Box 724
Wayne, IL 60184

Name & Address of Chief Elected Official of Municipality (Mayor/ Village President):

Eileen Phipps
PO Box 532
Wayne, IL 60184

Name & Address of County Board Chairperson:

Deborah Conroy
421 N County Farm Rd.
Wheaton, IL 60187